



*HRA Regular Meeting - After Council*  
*EDA Regular Meeting - After Council*  
*Council Closed Session - 6:30 p.m.*

## **CITY COUNCIL AGENDA**

**Tuesday, April 7, 2015**

**7:00 p.m.**

**Coon Rapids City Center  
Council Chambers**

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### **Call to Order**

### **Pledge of Allegiance**

### **Roll Call**

### **Adopt Agenda**

### **Proclamations/Presentations**

1. MN Office of Traffic Safety to Award Radar System for a Coon Rapids Squad Car
2. Oath of Officer for New Police Officers
3. Oath of Office New Firefighter Andy Peisert

### **Approval of Minutes of Previous Meeting**

4. Approve Minutes of March 17, 2015

### **Consent Agenda**

5. Approve Contract Between the State of Minnesota and the City of Coon Rapids for the ICWC Program
6. Approve Specifications for Boiler Replacement at City Hall and Order Advertisements for Bids
7. Approve Waiver of Fees for Fourth of July Celebration
8. Approve Temporary On-Sale Strong Beer and Wine Liquor License for Springfest at Epiphany Church
9. Announce 2015 Summer Concert Schedule
10. Approve On-Sale 3.2 Percent Malt Liquor, Wine, Strong Beer, and Sunday Liquor Licensing for Cheers Pablo, 12759 Riverdale Blvd

## **Public Hearing**

### **Bid Openings and Contract Awards**

11. Adopt Resolution No. 15-1(9) Awarding a Contract for Street Reconstruction - Project 15-1
12. Adopt Resolution No. 15-3(9) Awarding a Contract for Street Reconstruction - Project 15-3
13. Contract Award for 2015 Street Maintenance Program, Project 15-5

### **Old Business**

### **New Business**

14. PC 15-12 , Consider Approval of an Amendment to Gateway Commerce Center Planned Unit Development
15. Consider Approval of Master Contracting Agreement for Citywide Emergency Repairs
16. Consider Adopting Resolution 15-61 Year End Budget Appropriations
17. Consider Adopting Resolution 15-62 Transfer of Funds
18. PC 15-11, Consider Approval of Site Plan with Use Flexibility for Senior Housing, Crooked Lake Blvd and 109th, Coon Rapids Leased Housing Associates
19. PC 15-10, Consider Approval of Preliminary and Final Plat, Robinwood Plat 6, 109th Lane and Crooked Lake Blvd., Coon Rapids Leased Housing Assoc.
20. Consider Adopting Resolution 15-59 Calling for Public Hearing on Street and Easement Vacation, 110xx Crooked Lake Blvd.
21. Consider Resolution 15-60 Establishing Tax Increment Financing Plan for District 1-31
22. Consider Foley Boulevard Station Area Plan

### **Other Business**

### **Open Mic/Public Comment**

### **Reports on Previous Open Mic**

### **Adjourn**





**City Council Regular**

**1.**

**Meeting Date:** 04/07/2015

**Subject:** MN Office of Traffic Safety to Award Radar System for a Coon Rapids Squad Car

**From:** Brad Wise, Police Chief

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**INTRODUCTION**

In recognition for the hard work and commitment of the City of Coon Rapids and its police department to traffic safety, the Minnesota Office of Traffic Safety would like to present to the City a new radar to be placed into service in squad car. Bill Hammes, a representative of the Office of Traffic Safety, would like to make a formal presentation of this radar to the City.

**DISCUSSION**

The Coon Rapids PD has been a long time supporter of traffic safety initiatives of the MN Office of Traffic Safety. Most recently, the police department has assisted the Office of Traffic Safety by agreeing to be the fiscal agent for the 2014-2015 Anoka County grant. During the 4th quarter CRPD displayed leadership throughout Anoka County by hosting several of the planned events.

CRPD participates in the planned projects throughout Anoka County during the 4th quarter. CRPD support and participation at the monthly planning meetings. As recognition for these efforts the Office of Traffic Safety would like to award CRPD a Stalker Radar for installation in a Coon Rapids Squad car.

**RECOMMENDATION**

Staff recommends Council accept the award of a radar from the MN Office of Traffic Safety.

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**City Council Regular**

**2.**

**Meeting Date:** 04/07/2015

**Subject:** Oath of Officer for New Police Officers

**From:** Brad Wise, Police Chief

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**INTRODUCTION**

Mayor Koch will administer the Oaths of Office to Christopher Grew and Matthew Martell, new Police Officers for the City of Coon Rapids.

**DISCUSSION**

Chief Brad Wise will introduce Officers Grew and Martell to the Council and the Community. Both officers were hired in October 2014 and are now graduating from their Field Training Program. Part of the ceremony will be the pinning of their newly earned permanent police badges by someone each graduate has chosen. Mayor Koch will then administer their Oaths of Office.

**RECOMMENDATION**

Staff recommends Mayor Koch administer the Oath of Office to Christopher Grew and Matthew Martell.

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**BUDGET IMPACT:**

The hiring of both officers was done to fill vacancies caused by retirements. The Police Department is currently at its authorized full strength of 64 officers.

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**Attachments**

Grew oath of office

Martell oath of office

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# OATH OF OFFICE

STATE OF MINNESOTA )  
COUNTY OF ANOKA ) ss  
CITY OF COON RAPIDS )

I, Christopher Grew, do solemnly swear that I will support the Constitution of the United States, and the Constitution of the State of Minnesota, and discharge faithfully the duties of a Police Officer for the City of Coon Rapids, in the County of Anoka, and the State of Minnesota, to the best of my judgment and ability.

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Christopher Grew

WITNESSED:

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Jerry Koch, Mayor

Filed this 7<sup>th</sup> day of April, 2015.

# OATH OF OFFICE

STATE OF MINNESOTA )  
COUNTY OF ANOKA ) ss  
CITY OF COON RAPIDS )

I, Matthew Martell, do solemnly swear that I will support the Constitution of the United States, and the Constitution of the State of Minnesota, and discharge faithfully the duties of a Police Officer for the City of Coon Rapids, in the County of Anoka, and the State of Minnesota, to the best of my judgment and ability.

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Matthew Martell

WITNESSED:

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Jerry Koch, Mayor

Filed this 7<sup>th</sup> day of April, 2015.



**City Council Regular**

**3.**

**Meeting Date:** 04/07/2015

**Subject:** Oath of Office New Firefighter Andy Peisert

**From:** Joan Lenzmeier, City Clerk

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**INTRODUCTION**

Mayor Koch will administer the Oath of Office to New Firefighter Andy Peisert.

**DISCUSSION**

**RECOMMENDATION**

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**Attachments**

**Peisert Oath**

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**City Council Regular**

**4.**

**Meeting Date:** 04/07/2015

**SUBJECT:** Approve Minutes of March 17, 2015

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**Attachments**

March 17, 2015 Minutes

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## **UNAPPROVED**

### **COON RAPIDS CITY COUNCIL MEETING MINUTES OF MARCH 17, 2015**

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#### **CALL TO ORDER**

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The second regular meeting of the Coon Rapids City Council for the month of March was called to order by Mayor Jerry Koch at 7:00 p.m. on Tuesday, March 17, 2015, in the Council Chambers.

#### **PLEDGE OF ALLEGIANCE TO THE FLAG**

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Mayor Koch led the Council in the Pledge of Allegiance.

#### **ROLL CALL**

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Members Present: Mayor Jerry Koch, Councilmembers Ron Manning, Wade Demmer, Jennifer Geisler, Brad Johnson and Steve Wells

Members Absent: Councilmember Denise Klint

#### **ADOPT AGENDA**

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MOTION BY COUNCILMEMBER GEISLER, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT THE AGENDA AS AMENDED, MOVING ITEM 19 TO BE DISCUSSED FIRST UNDER NEW BUSINESS. THE MOTION PASSED UNANIMOUSLY.

#### **PROCLAMATIONS/PRESENTATIONS**

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##### **1. NUCLEUS CLINIC PRESENTATION**

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Becky Fink, Nucleus Clinic Executive Director, provided comment on the members of the community that were served by her clinic each year. She discussed the struggles the Nucleus Clinic have occurred over the past several years as a non-profit. She appreciated the City's assistance with providing the clinic with a \$40,000 interest free loan. She was proud to report the Nucleus Clinic would be making its last debt payment this evening. She invited the Council to attend an Open House at the Nucleus Clinic on Thursday, April 23<sup>rd</sup>.

#### **APPROVAL OF MINUTES OF PREVIOUS MEETINGS**

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##### **2. MARCH 3, 2015, COUNCIL MEETING**

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MOTION BY COUNCILMEMBER JOHNSON, SECONDED BY COUNCILMEMBER GEISLER, FOR APPROVAL OF THE MINUTES OF THE MARCH 3, 2015, COUNCIL MEETING. THE MOTION PASSED 5-0-1 (WELLS ABSTAINED).

CONSENT AGENDA/INFORMATIONAL BUSINESS

3.      ADOPT RESOLUTION 15-50 AND 15-51, ADOPTING MISCELLANEOUS SPECIAL ASSESSMENTS
4.      APPROVE THERAPEUTIC MASSAGE LICENSE FOR IBIS SALON AND SPA, 1818 COON RAPIDS BOULEVARD
5.      ADOPT RESOLUTION 15-53, ACCEPTING COON RAPIDS LIONS CLUB DONATION
6.      ADOPT RESOLUTION 15-56, AMENDING TERM OF OFFICE FOR RONALD BRADLEY ON BOARD OF ADJUSTMENT AND APPEALS

Finance Director Legg reviewed the amended miscellaneous special assessment amounts with the Council, noting staff had received several payments.

MOTION BY COUNCILMEMBER MANNING, SECONDED BY COUNCILMEMBER DEMMER, FOR APPROVAL OF THE CONSENT AGENDA AS AMENDED. THE MOTION PASSED UNANIMOUSLY.

PUBLIC HEARING

None.

BID OPENINGS AND CONTRACT AWARDS

7.      CONSIDER ADOPTING RESOLUTION NO. 13-21(9) AWARDING A CONTRACT FOR WILDERNESS TRAIL CONSTRUCTION

The Staff report was shared with Council.

Mayor Koch asked when this project would begin. Public Works Director Himmer reported that the project would be completed this summer through July in order to receive the grant funds.

Councilmember Manning clarified that the City's portion of this project was \$73,000 and the remainder of the project would be paid for by a grant.

MOTION BY COUNCILMEMBER WELLS, SECONDED BY COUNCILMEMBER MANNING, TO ADOPT RESOLUTION NO. 13-21(9) AWARDING A CONTRACT TO BARBER



CONSTRUCTION, INC. IN THE AMOUNT OF \$121,240.50 FOR CONSTRUCTION OF WILDERNESS TRAIL. THE MOTION PASSED UNANIMOUSLY.

#### OLD BUSINESS

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None.

#### NEW BUSINESS

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##### 7A.    CONSIDER SALE OF \$10,000,000 GENERAL OBLIGATION BONDS, SERIES 2015A

The Staff report was shared with Council.

Nick Anhut, Ehlers & Associates, discussed the bond sale and noted the City had an AA1 bond rating. It was noted this bond rating assisted the City with receiving favorable results on the bond sale. He explained the City received eight bids for its current bond sale with the winning bid was received from Wells Fargo with an interest rate of 1.9%. He recommended the Council move forward with the bond sale to Wells Fargo.

MOTION BY COUNCILMEMBER JOHNSON, SECONDED BY COUNCILMEMBER WELLS, TO ADOPT RESOLUTION NO. 15-55 AWARDING THE SALE OF \$10,000,000 GENERAL OBLIGATION BONDS, SERIES 2015A FIXING THEIR FORM AND SPECIFICATIONS; DIRECTING THEIR EXECUTION AND DELIVERY; AND PROVIDING FOR THEIR PAYMENT. THE MOTION PASSED UNANIMOUSLY.

##### 8.      PC15-8: CONSIDER APPROVAL OF A RESOLUTION APPROVING A CHANGE IN THE LAND USE FROM OFFICE TO COMMUNITY COMMERCIAL, 80 COON RAPIDS BOULEVARD

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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER MANNING, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT A RESOLUTION APPROVING THE PROPOSED LAND USE AMENDMENT BASED ON THE FOLLOWING FINDINGS:

1. THE PROPOSED AMENDMENT IS CONSISTENT WITH THE COMPREHENSIVE PLAN IN THAT IT PROMOTES THE IMPROVED APPEARANCE AND FUNCTION OF COON RAPIDS BOULEVARD.

2. THE PROPOSED LAND USE AMENDMENT IS COMPATIBLE WITH THE SURROUNDING LAND USE DESIGNATIONS AND LAND USES.
3. THE PROPOSED LAND USE AMENDMENT WOULD NOT HAVE AN ADVERSE IMPACT ON THE AREA.
4. THE PARCEL FRONTS ON A COON RAPIDS BOULEVARD, WHICH IS A CLASS A ARTERIAL STREET.

THE MOTION PASSED UNANIMOUSLY.

9.     PC15-3: CONSIDER APPROVAL OF RESOLUTION APPROVING A LAND USE AMENDMENT CHANGING THE LAND USE FROM MODERATE DENSITY RESIDENTIAL TO GENERAL COMMERCIAL
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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER GEISLER, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT A RESOLUTION APPROVING THE PROPOSED LAND USE CHANGE CHANGE FROM MODERATE DENSITY RESIDENTIAL TO GENERAL COMMERCIAL WITH THE FOLLOWING FINDINGS:

1. THE PROPOSED AMENDMENT IS CONSISTENT WITH THE COMPREHENSIVE PLAN IN THAT IT PROMOTES THE EXPANSION OF AN EXISTING BUSINESS WHICH WILL INCREASE EMPLOYMENT OPPORTUNITIES AND INCREASE THE CITY'S TAX BASE.
2. THE PROPOSED LAND USE AMENDMENT IS COMPATIBLE WITH THE SURROUNDING LAND USE DESIGNATIONS AND LAND USES.
3. THE PROPOSED LAND USE AMENDMENT WOULD NOT HAVE AN ADVERSE IMPACT ON THE AREA.
4. WHEN COMBINED WITH THE ABUTTING PROPERTY, THE PARCEL WILL HAVE FRONTAGE ON HIGHWAY 10.

Councilmember Geisler was pleased that this site would have a new use.

THE MOTION PASSED UNANIMOUSLY.

10.    CONSIDER RECOMMENDATION FOR REAPPOINTMENT OF CHARTER COMMISSION MEMBERS AND RESIGNATION
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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER DEMMER, SECONDED BY COUNCILMEMBER WELLS, TO RECOMMEND TO THE CHIEF JUDGE FOR THE TENTH JUDICIAL DISTRICT THAT MAURICE MCKEE, ISLA FICHTNER, GERALD NELSON, MILO HARTMANN, CAROLINE LACOURSIERE, GERALD SPLINTER, TIMOTHY FARMER AND ELIZABETH DHENNIN BE REAPPOINTED TO SERVE ANOTHER FOUR-YEAR TERM EFFECTIVE APRIL 2, 2015 WITH SAID TERM TO EXPIRE MARCH 31, 2019; AND TO ACCEPT THE RESIGNATION OF THOMAS MILLER.

Mayor Koch thanked Thomas Miller for his service on the Charter Commission.

THE MOTION PASSED UNANIMOUSLY.

11.     CONSIDER RESOLUTION 15-42 SUPPORTING SPECIAL LEGISLATION RELATED  
          TO TAX INCREMENT FINANCING DISTRICT 6-1

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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER JOHNSON, SECONDED BY COUNCILMEMBER DEMMER, TO ADOPT RESOLUTION NO. 15-42 SUPPORTING SPECIAL LEGISLATION RELATED TO TIF DISTRICT 6-1. THE MOTION PASSED UNANIMOUSLY.

12.     CONSIDER WATER RATE INCREASE

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The Staff report was shared with Council.

Mayor Koch questioned why Blaine's water rates were half that of Coon Rapids. City Manager Gatlin reported Blaine does not complete a manganese treatment on their water, which lowered their overall rates.

Mayor Koch was pleased that the new water rates would have a higher base rate to assist the City in maintaining its infrastructure long term.

MOTION BY COUNCILMEMBER WELLS, SECONDED BY COUNCILMEMBER GEISLER, TO ADOPT RESOLUTION 15-52 NO. ESTABLISHING WATER RATES. THE MOTION PASSED UNANIMOUSLY.

13.      **CONSIDER ADOPTING RESOLUTION NO. 15-54 SUPPORTING DEDICATED STATE FUNDING FOR CITY STREETS**

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The Staff report was shared with Council.

**MOTION BY COUNCILMEMBER MANNING, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT RESOLUTION NO. 15-54 SUPPORTING DEDICATED STATE FUNDING FOR CITY STREETS.**

Councilmember Demmer questioned how the City would be impacted by this Resolution. Public Works Director Himmer explained the Resolution would assist cities in creating more options for street funding through rededication of State funds. He reported that cities had a need to repair streets that was greater than the amount that was currently being funded through MSA funds. Finance Director Legg indicated that State Aid streets were in need of repair at this time.

**THE MOTION PASSED UNANIMOUSLY.**

14.      **CONSIDER ADOPTING RESOLUTION NO. 15-11(8) APPROVING PLANS AND SPECS AND ORDERING ADVERTISEMENT FOR BID FOR MISCELLANEOUS BITUMINOUS REPAIR CONTRACT**

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The Staff report was shared with Council.

**MOTION BY COUNCILMEMBER WELLS, SECONDED BY COUNCILMEMBER DEMMER, TO ADOPT RESOLUTION NO. 15-22(8) APPROVING PLANS AND SPECIFICATIONS AND ORDERING ADVERTISEMENT FOR BIDS FOR THE MISCELLANEOUS BITUMINOUS REPAIR CONTRACT. THE MOTION PASSED UNANIMOUSLY.**

15.      **CONSIDER 2015 SCORE GRANT APPROVAL**

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The Staff report was shared with Council.

City Recycling Coordinator Sinclair discussed the City's recycling program with the Council and noted Coon Rapids received a SCORE Grant from the State through Anoka County. She reviewed the growing success of the City's recycling program over the past ten years. She provided comment on how the City was working to achieve the new recycling standards laid out by the County. Staff requested the Council approve new hours for the Recycling Assistant through and approve an amended SCORE funding recycling agreement with Anoka County.

Public Works Director Himmer reported that the City would have no additional expenses for this position in 2015. He noted that the City would be obligated to offer health care to this part time employee in 2016.

Councilmember Johnson asked if a PERA contribution would be required for this employee. Public Works Director Himmer commented this would be an additional expense to the City.

Councilmember Geisler believed the City's recycling program was a great success and she supported the proposed request.

MOTION BY COUNCILMEMBER GEISLER, SECONDED BY COUNCILMEMBER MANNING, TO APPROVE THE SCORE FUNDING RECYCLING AGREEMENT BETWEEN ANOKA COUNTY AND THE CITY OF COON RAPIDS; RECOMMENDING THE RECYCLING ASSISTANT'S HOURS BE EXPANDED TO 32 HOURS/WEEK IN 2015. THE MOTION PASSED UNANIMOUSLY.

16.    PC 15-9 ZONE CHANGE: CONSIDER ADOPTION OF ORDINANCE 2135 CHANGING THE ZONING FROM OFFICE TO COMMUNITY COMMERCIAL, 80 COON RAPIDS BOULEVARD

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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER MANNING, SECONDED BY COUNCILMEMBER GEISLER, TO ADOPT ORDINANCE 2135 APPROVING THE REZONING BASED ON THE FOLLOWING FINDINGS:

1. THE PROPOSED REZONING TO COMMUNITY COMMERCIAL IS CONSISTENT WITH THE LAND USE DESIGNATION OF COMMUNITY COMMERCIAL.
2. THE PROPOSED REZONING IS COMPATIBLE WITH THE ADJACENT LAND USES AND ZONING.
3. THE TIMES AND CONDITIONS HAVE CHANGE SO THAT A REASONABLE USE OF THE PROPERTY CAN NOT BE MADE UNDER THE CURRENT ZONING.
4. THE PROPOSED ZONE CHANGE WOULD NOT HAVE AN ADVERSE IMPACT ON THE AREA.
5. THE PROPOSED REZONING IS CONSISTENT WITH THE COON RAPIDS BOULEVARD FRAMEWORK PLAN AND THE RIVER RAPIDS OVERLAY DISTRICT.

THE MOTION PASSED UNANIMOUSLY.

17. PC15-4: CONSIDER ADOPTION OF ORDINANCE 2136 TO CHANGE ZONING FROM MODERATE DENSITY RESIDENTIAL TO GENERAL COMMERCIAL

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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER GEISLER, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT ORDINANCE 2136 APPROVING THE PROPOSED ZONE CHANGE FROM OFFICE TO GENERAL COMMERCIAL WITH THE FOLLOWING FINDINGS:

1. THE PROPOSED REZONING TO GENERAL COMMERCIAL IS CONSISTENT WITH THE LAND USE DESIGNATION OF GENERAL COMMERCIAL.
2. THE PROPOSED REZONING IS COMPATIBLE WITH THE ADJACENT LAND USES AND ZONING.
3. THE PROPOSED REZONING IS THE LOGICAL EXTENSION OF THE ADJACENT GENERAL COMMERCIAL DISTRICT.
4. THE TIMES AND CONDITIONS HAVE CHANGE SO THAT A REASONABLE USE OF THE PROPERTY CAN NOT BE MADE UNDER THE CURRENT ZONING.
5. THE PROPOSED ZONE CHANGE WOULD NOT HAVE AN ADVERSE IMPACT ON THE AREA.

THE MOTION PASSED UNANIMOUSLY.

18. CONSIDER ADOPTION OF ORDINANCE 2134 ESTABLISHING FEE FOR TEMPORARY ON SALE INTOXICATING LIQUOR (STRONG BEER AND WINE) LICENSE

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The Staff report was shared with Council.

MOTION BY COUNCILMEMBER MANNING, SECONDED BY COUNCILMEMBER WELLS, TO ADOPT ORDINANCE 2134 ESTABLISHING THE LICENSE FEE FOR TEMPORARY ON SALE INTOXICATING LIQUOR LICENSE FOR STRONG BEER AND WINE.

Councilmember Manning recommended that the City allow for the tasting of wine and beer without requiring a bartender. City Clerk Lenzmeier explained that if the applicant met all requirements within the City's liquor license a bartender would not be required.

THE MOTION PASSED UNANIMOUSLY.

20.      **CONSIDER ADOPTION OF ORDINANCE 2130 REGARDING CHAPTER 8-1200 –  
SURFACE WATER MANAGEMENT**

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The Staff report was shared with Council.

Councilmember Johnson noted this matter was tabled at the March 3, 2015 City Council meeting. He reviewed his concerns with the language within the Ordinance and thanked staff for working with him to address these concerns.

**MOTION BY COUNCILMEMBER DEMMER, SECONDED BY COUNCILMEMBER JOHNSON, TO ADOPT ORDINANCE 2130, UPDATING THE ORDINANCE FOR SURFACE WATER MANAGEMENT, AND AMEND THE CITY CODE ACCORDINGLY. THE MOTION PASSED UNANIMOUSLY.**

**OPEN MIC/PUBLIC COMMENT**

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Mayor Koch reviewed the rules of order for the Open Mic/Public Comment portion of the meeting.

**REPORTS ON PREVIOUS OPEN MIC**

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None.

**OTHER BUSINESS**

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Mayor Koch requested comment on the exciting news at the golf course. Bunker Hills Golf Director Anderson indicated the driving range and golf course was open, which was the earliest opening in the history of the golf course. He reported that the response has been overwhelming both on the course and at the restaurant.

Police Chief Wise discussed a recent cardiac arrest event that occurred at the Coon Rapids Ice Arena. He stated that the training provided by Officer Platz assisted in this AED save. Mayor Koch thanked the Police Department for their assistance in creating a Heart Safe Community.

Mayor Koch provided comment on a railway transportation summit that was held in the City last December. He reported that the Hanson Boulevard grade crossing was now being considered by the governor for funding, which was a great step for the City.

ADJOURN

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MOTION BY COUNCILMEMBER JOHNSON, SECONDED BY COUNCILMEMBER WELLS,  
TO ADJOURN THE MEETING AT 8:13 P.M. THE MOTION PASSED UNANIMOUSLY.

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Jerry Koch, Mayor

ATTEST:

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Joan Lenzmeier, City Clerk





**City Council Regular**

**5.**

**Meeting Date:** 04/07/2015

**Subject:** Approve Contract Between the State of Minnesota and the City of Coon Rapids for the ICWC Program

**Submitted For:** Tim Himmer, Public Works Director

**From:** Kim Reid, Administrative Legal Assistant

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**INTRODUCTION**

Every two years the City considers a contract between the State of Minnesota Department of Corrections and the City of Coon Rapids for services to provide an Institution Community Work Crew (ICWC) to complete various maintenance and construction projects for the City. A contract has been prepared by Terry Byrne, the ICWC District Supervisor, for review and approval by the City.

**DISCUSSION**

The ICWC crew has generally worked under the supervision of the Public Works Department supervisory staff and has completed a wide variety of projects for the City. They have been a valuable asset to the community and have provided thousands of hours of labor at a very reasonable cost.

The proposed contract fee would be \$82,511.00 per year for the two-year contract period. Funding for this program is included in various Public Works budgets including Streets, Parks, Vehicle Maintenance, Sewer, Water and Storm Water. The contract period would run from April 1, 2015 to March 31, 2017.

Staff believes this program provides cost effective maintenance services to supplement Public Works personnel. Staff recommends Council consider approval of the two-year contract to continue the ICWC program.

**RECOMMENDATION**

It is recommended the City Council approve the contract between the Minnesota Department of Corrections and the City of Coon Rapids for the two-year ICWC Program.

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**BUDGET IMPACT:**

Funding for the ICWC Program is included in various Public Works operating budgets. Funding for the remaining portion of 2015 has been included in the 2015 Budget. Funding for 2016 and the first three months of 2017 will be allocated in future budgets based on the proposed contract amounts.

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**Attachments**

**ICWC Contract**

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## STATE OF MINNESOTA INCOME CONTRACT

This contract is between the State of Minnesota, acting through its commissioner of corrections, Institution Community Work Crew Program ("State"), and the City of Coon Rapids, 11155 Robinson Drive Northwest, Coon Rapids, Minnesota 55423 ("Purchaser").

### Recitals

1. Under Minn. Stat. §241.278 the State is empowered to enter into income contracts.
2. The Purchaser is in need of an Institution Community Work Crew (ICWC).
3. The State represents that it is duly qualified and agrees to provide the services described in this contract.

### Contract

#### 1 Term of Contract

- 1.1 **Effective date:** April 1, 2015, or the date the State obtains all required signatures under Minnesota Statutes Section 16C.05, subdivision 2, whichever is later.
- 1.2 **Expiration date:** March 31, 2017, or until all obligations have been satisfactorily fulfilled, whichever occurs first.

#### 2 State's Duties

The State will:

- A. Provide a supervisor crew leader(s) and up to ten (10) offender crewmembers that the crew leader will supervise during four (4) 10-hour work days per week, including the hours crew leaders spend for daily preparation, communication and travel. The crew leader will take directions as to the location and nature of the work to be completed on a given day as requested by the Purchaser's Authorized Representative or designee.
- B. Train each work crew in safety principles and techniques set forth by applicable federal, state and local agency requirements. Purchaser agrees that the state has the authority to refuse selected projects if it considers the projects beyond the skill level of the crewmembers and/or unsafe to perform.
- C. Provide required personal safety equipment and clothing needed for specific work.
- D. Screen projects to ensure that appropriate staff are assigned.
- E. Submit reports to the Purchaser upon request within sixty (60) days of the end of each quarter.

#### 3 Payment

The Purchaser will pay the State for all services performed by the State under this contract as follows: Payment shall be made by the Purchaser to the State in the amount of forty-one thousand two hundred fifty-five and 50/100 dollars (\$41,255.50) on May 1, 2015, forty-one thousand two hundred fifty-five and 50/100 dollars (\$41,255.50) on November 1, 2015, forty-one thousand two hundred fifty-five and 50/100 dollars (\$41,255.50) on May 1, 2016 and, forty-one thousand two hundred fifty-five and 50/100 dollars (\$41,255.50) on November 1, 2016. Any overtime hours will be billed at the rate of sixty-seven and 50/100 dollars (\$67.50) per hour.

The total obligation of the Purchaser for all compensation and reimbursements to the State under this contract is one hundred sixty-five thousand twenty-two and 00/100 dollars (\$165,022.00), plus any additional overtime hours, as its share of the cost of providing a crew leader and placing the work crews into service on the ICWC Program during the term of this agreement. The Purchaser's share includes time scheduled for training, vacation, sick leave and holidays based on the terms and condition of the crew leaders AFSCME bargaining agreement.

**4 Authorized Representatives**

The State's Authorized Representative is Terry Byrne, District Supervisor, 1450 Energy Park Drive, Suite 200, St. Paul, MN 55108 or his successor.

The Purchaser's Authorized Representative is Tim Himmer, Director of Public Works, City of Coon Rapids, 11155 Robinson Drive Northwest, Coon Rapids, MN 55423 or his successor.

**5 Amendments, Waiver, and Contract Complete**

- 5.1 **Amendments.** Any amendment to this contract must be in writing and will not be effective until it has been executed and approved by the same parties who executed and approved the original contract, or their successors in office.
- 5.2 **Waiver.** If the State fails to enforce any provision of this contract, that failure does not waive the provision or its right to enforce it.
- 5.3 **Contract Complete.** This contract contains all negotiations and agreements between the State and the Purchaser. No other understanding regarding this contract, whether written or oral, may be used to bind either party.

**6 Liability**

Each party will be responsible for its own acts and omissions and results thereof to the extent authorized by the law and shall not be responsible for the acts and omissions of the other party, their employees or agents and the results thereof. The State's liability shall be governed by the provisions of the Minnesota Tort Claims Act, Minnesota Statutes 3.736, and other applicable law. The City's liability shall be governed by the provisions of the Municipal Tort Claims Act, Minnesota Statute Chapter 466 and other applicable law.

**7 Government Data Practices**

The Purchaser must comply with the Minnesota Government Data Practices Act, Minn. Stat. Ch. 13, as it applies to all data provided by the State under this contract. The civil remedies of Minn. Stat. § 13.08 apply to the release of the data referred to in this clause by either the Purchaser or the State.

If the Purchaser receives a request to release the data referred to in this Clause, the Purchaser must immediately notify the State. The State will give the Purchaser instructions concerning the release of the data to the requesting party before the data is released.

**8 Publicity**

Any publicity regarding the subject matter of this contract must not be released without prior written approval from the State's Authorized Representative.

**9 Audit**

Under Minn. Stat. § 16C.05, subd. 5, the Purchaser's books, records, documents, and accounting procedures and practices relevant to this contract are subject to examination by the State and/or the State Auditor or Legislative Auditor, as appropriate, for a total of six years.

**10 Governing Law, Jurisdiction, and Venue**

Minnesota law, without regard to its choice-of-law provisions, governs this contract. Venue for all legal proceedings out of this contract, or its breach, must be in the appropriate state or federal court with competent jurisdiction in Ramsey County, Minnesota.

**11 Termination**

Either party may terminate this agreement at any time, with or without cause, upon 30 days' written notice to the other party.

**1. PURCHASER**

The Purchaser certifies that the appropriate person(s) have executed the contract on behalf of the Purchaser as required by applicable articles, bylaws, resolutions, or ordinances.

By
Title
Date

**APPROVAL AS TO FORM:**

By
Title
Date

**2. STATE AGENCY**

With delegated authority

By
Title
Date

**3. Commissioner of Administration**

As delegated to Materials Management Division

By
Date

Distribution  
DOC Financial Services Unit – Original (fully executed) contract  
Purchaser  
State's Authorized Representative  
Budget Officer of Authorized Representative  
Department of Administration – Materials Management Division



**City Council Regular**

**6.**

**Meeting Date:** 04/07/2015

**Subject:** Approve Specifications for Boiler Replacement at City Hall and Order Advertisements for Bids

**Submitted For:** Sharon Legg, Finance Director

**From:** Traci Reimringer, Purchasing Clerk

---

**INTRODUCTION**

Staff is requesting City Council authorization to obtain bids for the replacement of boilers at City Hall and order the advertisement for bids to start the bidding process.

**DISCUSSION**

Staff is recommending the replacement of the boilers at City Hall. It is anticipated that three to five high efficiency boilers will be required. The new units will be compatible and integrated with the current UHL Building Control system.

New Minnesota State regulations require that the City solicit separately for equipment as well as labor on purchases such as the boiler replacement project in order to take full advantage of the tax exemption status. Because of this requirement, advertisement for bids will be listed for the equipment and materials of the boilers and then a separate advertisement for bids will be listed for the labor and installation of the boilers. Therefore, there is a potential to award contracts to two separate vendors for this project.

The 2014 Budget included \$175,000 for this expenditure in the Facility Construction Fund which is available for 2015.

**RECOMMENDATION**

Approve the specifications for the boiler replacement and order the advertisement for bids to be opened on April 30, 2015.

---



**City Council Regular**

7.

**Meeting Date:** 04/07/2015

**Subject:** Approve Waiver of Fees for Fourth of July Celebration

**Submitted For:** Joan Lenzmeier, City Clerk

**From:** Nathalie O'Shaughnessy, Admin 1

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**INTRODUCTION**

Chief Piper has submitted a request to waive licensing fees for the 2015 Fourth of July celebration.

**DISCUSSION**

Chief Piper, on behalf of the Coon Rapids Fire Relief Association and the Coon Rapids North Star Lions, has submitted a request to waive the fireworks display, carnival, and parade fees for the annual Fourth of July celebration. Historically these fees have been waived for this City event.

While the Clerk's Office has the authority to issue the licenses, any fee adjustment must be approved by Council.

**RECOMMENDATION**

Council is requested to approve the waiver of the fireworks display, carnival and parade license fees for the 2015 Fourth of July celebration.

---

**Attachments**

Chief Piper

---

11155 Robinson Drive  
Coon Rapids MN 55433  
Tel 763-755-2880  
Fax 763-767-6491  
www.coonrapidsmn.gov



# COON RAPIDS Minnesota

TO: Coon Rapids City Council  
FROM: John Piper, Fire Chief  
SUBJECT: Carnival Fees  
DATE: April 1, 2015

A handwritten signature in black ink, appearing to be 'JP' or 'John Piper', is located to the right of the 'FROM:' line.

The Coon Rapids Fire Relief Association and the Coon Rapids North Star Lions will be hosting the annual Fourth of July celebration at Boulevard Park, adjacent to the Coon Rapids Ice Center (CRIC), July 2nd - 4th.

We respectfully request the City Council consider waiving the licensing fees for the carnival, fireworks display and parade. This celebration brings the community together and provides quality family entertainment.

Thank you for your consideration.



**City Council Regular**

**8.**

**Meeting Date:** 04/07/2015

**Subject:** Approve Temporary On-Sale Strong Beer and Wine Liquor License for Springfest at Epiphany Church

**Submitted For:** Stephanie Lincoln, Deputy City Clerk

**From:** Stephanie Lincoln, Deputy City Clerk

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**INTRODUCTION**

Jay Tomaszewski, on behalf of Epiphany Church, 1900 111th Avenue NW, has applied for a temporary on-sale strong beer and wine liquor license to be used at Springfest on May 15th, 16th, and 17th 2015.

**DISCUSSION**

The appropriate fees have been paid and the Certificate of Insurance for liquor liability is on file, and Mr. Tomaszewski's background investigation has been completed and the Police Department has given approval.

The temporary on-sale strong beer and wine liquor license will be valid from 5:00 to 10:30 p.m. on Friday, May 15th; 9:00 a.m. to 10:30 p.m. on Saturday, May 16th; and noon to 4:00 p.m. on Sunday, May 17th.

**RECOMMENDATION**

Council approval of a temporary strong beer and wine liquor license for Epiphany Church for use at Springfest on May 15th, 16th, and 17th, 2015.

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**City Council Regular**

**9.**

**Meeting Date:** 04/07/2015

**Subject:** Announce 2015 Summer Concert Schedule

**Submitted For:** Stephanie Lincoln, Deputy City Clerk

**From:** Stephanie Lincoln, Deputy City Clerk

---

**INTRODUCTION**

The Arts Commission is pleased to announce the 2015 Summer concert series at the Coon Rapids Dam Regional Park.

**DISCUSSION**

The Arts Commission would like to announce their 2015 Thursday Nites LIVE! At The Dam Summer series, which will feature the following nine performers:

June 4 - Gravel Roads (Country)  
June 11 - Jumpin' Jehosafats (Dixieland/Glspel/Jazz)  
June 18 - Backyard Band (Variety)  
June 25 - The High 48s (Bluegrass)  
July 9 - DyVersaCo Band (Polka)  
July 16 - Sam Kuusisto (R&B/FUnk/Jazz)  
July 23 - Sonny Winberg Band (Country)  
July 30 - Island Time Steel Drum Band (Steel Drum)  
Aug 6 - Buddy & The Boys (Buddy Holly Tribute Band - 50's)

All performances begin at 7:00 p.m. at the Performance Pavilion at the Coon Rapids Dam Regional Park. The concerts are offered at no charge to the community, and Anoka County has once again agreed to waive the parking fee during these evenings. The Arts Commission would like to recognize the assistance of the Anoka County Parks and Recreation Department Rangers and other staff in presenting this series.

A local boy scout troop will sell popcorn, candy, soda, and water during the concerts. The Northstar Lions Club will be selling food items such as hamburgers, hot dogs, and brats.

The Arts Commission appreciates the opportunity to continue presenting this series to the community.

**RECOMMENDATION**

In addition to being available at City Hall, the 2015 schedule has been distributed via utility bill mailings and will be in the City newsletter and website.

---



**City Council Regular**

**10.**

**Meeting Date:** 04/07/2015

**Subject:** Approve On-Sale 3.2 Percent Malt Liquor, Wine, Strong Beer, and Sunday Liquor Licensing for Cheers Pablo, 12759 Riverdale Blvd

**Submitted For:** Stephanie Lincoln, Deputy City Clerk

**From:** Stephanie Lincoln, Deputy City Clerk

---

**INTRODUCTION**

Deborah Magelssen, for Creative Clinic, Inc DBA Coon Rapids Cheers Pablo, has submitted applications for On-Sale 3.2 Percent Malt Liquor, Wine, Strong Beer, and Sunday Liquor licensing for Coon Rapids Cheers Pablo at 12759 Riverdale Blvd.

**DISCUSSION**

Creative Clinic, Inc has secured a lease for the space at 12759 Riverdale Blvd in order to open Coon Rapids Cheers Pablo as an art instruction, spirits and limited dinning establishment. The tentative plan is to open the establishment on April 15, 2015.

The license and investigation fees have been paid. The Police Department has conducted a comprehensive background investigation on Magelssen and have found nothing to prevent licensing.

Certificate of Insurance evidencing liquor liability and workers' compensation coverage have been received. Approval of the license is conditioned upon a Certificate of Occupancy and the Anoka County Environmental Services issuing a food service license.

**RECOMMENDATION**

Council is requested to approve the issuance of the On-Sale 3.2 Percent Malt Liquor, Wine, Strong Beer, and Sunday Liquor licensing to Creative Clinic, Inc DBA Coon Rapids Cheers Pablo at 12759 Riverdale Blvd., effective as of April 15, 2015, contingent upon obtaining a Certificate of Occupancy and a County food service license.

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## **City Council Regular**

**11.**

**Meeting Date:** 04/07/2015

**Subject:** Adopt Resolution No. 15-1(9) Awarding a Contract for Street Reconstruction - Project 15-1

**Submitted For:** Mark Hansen, Assistant City Engineer

**From:** Cher Ridout, Admin Secretary II

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### **INTRODUCTION**

The City is proposing reconstruction of 2.8 miles of residential streets in an area generally lying northwest of Hanson Boulevard and Main Street, an area north of 121st Avenue and east of Hanson Boulevard, an area south of 121st Avenue and east of the BNSF Railroad track, and an area south of Foley Boulevard between Kumquat Street and Holly Circle. Bids were received on March 20, 2015 for the improvement project. Council is requested to award a contract and adopt the assessment.

### **DISCUSSION**

The City is proposing to reconstruct approximately 2.8 miles of residential streets. Proposed improvements include reclaiming existing bituminous surface and aggregate base, removal and replacement of damaged curb and gutter, sidewalk and pedestrian curb ramp improvements in accordance with ADA requirements, replacement of watermain valves and hydrants, and repairs or replacements of the existing storm sewer as needed.

Two neighborhood meetings were held to inform residents on the project and answer any questions. The meetings were held on October 30, 2014 and January 22, 2015. City Council held a public hearing and assessment hearing for the project on February 17, 2015. Benefiting properties include single-family, multi-family, and condominium properties. The proposed assessment rate is \$1,803.00 for single-family residential properties, and \$22.53 per front foot for multi-family residential properties. The assessments would be levied over a 10-year period at an interest rate of 3.04%.

Prior to the assessment hearing, one property owner (June Evens at 916 120th Avenue NW) contested the assessment via a letter mailed to the City. Staff noted the objection to the Council as part of the project presentation prior to the hearing. Staff followed up immediately after the hearing with Ms. Evens to explain the process for formally contesting the assessment. Ms. Evens then later declined to pursue the objection any further. This was again confirmed by staff on March 30 via a phone call to Ms. Evens.

Also at the assessment hearing, Melvin Shulte at 1700 125th Lane contested the fact that he was being assessed for two properties within the project area. The PINs for these two properties are 03-31-24-44-0031 and 03-31-24-44-0032. Both properties were proposed to be assessed at the single-family rate of \$1,803.00. Mr. Shulte explained that one property includes his house structure (PIN 03-31-24-44-0031), while the other property includes only a pool and shed (PIN 03-31-24-44-0032). At the assessment hearing, it was discussed that Mr. Shulte would pursue a lot combination so that he would only have to pay one \$1,803.00 assessment. Staff have since worked with Mr. Shulte and Anoka County, and the lot combination process has been confirmed to be underway. PIN 03-31-24-44-0032 will be removed from the assessment roll. The new PIN for Mr. Shulte's combined property was confirmed by Anoka County on March 26, 2015 to be 03-31-24-44-0072.

Five bids were received on March 20, 2015, and are summarized as follows:

<b>Contractor</b>	<b>Bid Amount</b>
North Valley, Inc.	\$1,625,486.65
Northwest Asphalt	\$1,667,011.00
Hardrives	\$1,692,418.38
Asphalt Surface Technologies	\$1,828,233.10
Park Construction	\$1,988,366.06
Engineer's Estimate	\$1,631,713.75

The low bidder, North Valley, Inc., has completed a number of contracts with the City in the past, and has done a good job. If Council approves the contract on April 7, 2015, work can begin in May, and is anticipated to be complete in September 2015.

As part of the project, the City was required to get a watershed permit from the Coon Creek Watershed District for the repair of two broken culvert end sections. During the watershed's permit review process, additional stream bank protection was proposed to be included in the project, and to be performed directly adjacent to the planned culvert repair work. The stream bank protection work was included in the project bid as an alternate. The low bidder proposed a cost of \$5,631.05 to perform this work. The engineer's estimate was \$6,600.00. Since the low bidder's proposed cost for this alternate was close to the expected estimate, and is necessary to stabilize the Sand Creek stream bank in this area, staff recommends including the work within the contract total. The watershed has proposed to share in a portion of the cost for this work. Staff will work with the watershed to determine what the final cost sharing agreement will consist of.

### **RECOMMENDATION**

It is recommended the Council take the following action:

- a. Adopt Resolution No. 15-1(9) awarding a contract to North Valley, Inc. in the total project cost of \$1,625,486.65.
- b. Adopt Resolution No. 15-1(12) adopting the assessment, with the previously described deletion.

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### **BUDGET IMPACT:**

The construction cost for this project is \$1,625,486.65. Adding 20% to the construction costs for contingency and engineering services would bring the total estimated project cost to \$1,950,583.98. A total of \$536,423.40 is proposed to be assessed to benefiting properties. The balance of the project costs would be recovered from various funds. The amount of \$480,168.85 would be recovered from the Street Reconstruction Fund (797), \$48,321.60 would come from the Sanitary Sewer Fund (620), \$82,881.14 would come from the Storm Water Drainage Fund (640) recovered through storm drainage charges, and \$477,691.66 would come from the Water System Maintenance Fund (601) recovered through charges for water used.

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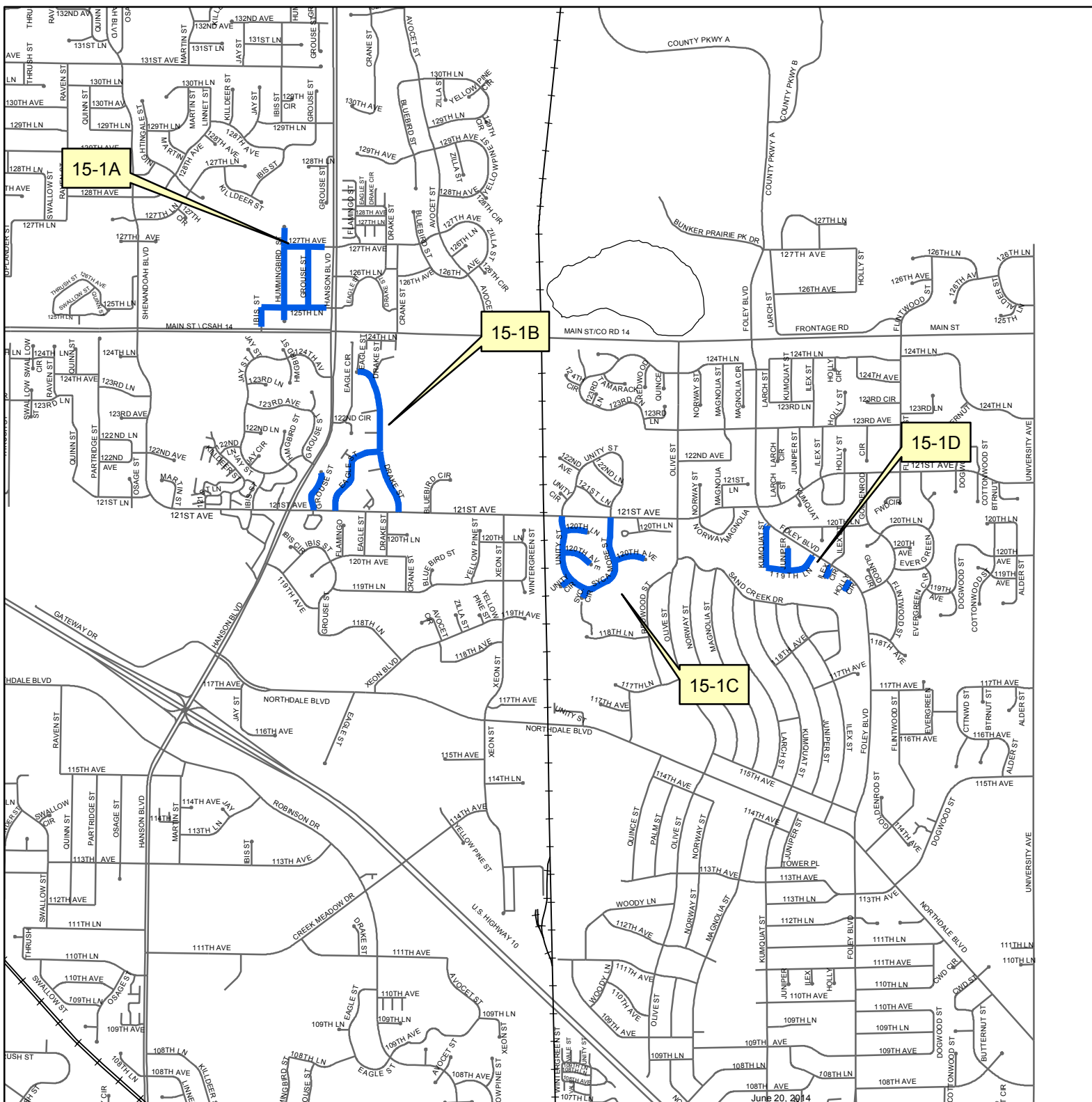
### **Attachments**

Location Map

Resolution 15-1(9)

Resolution 15-1(12)

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## 2015 Proposed Street Reconstruction Project 15-1

15-1 Residential Reconstruction 2.8 miles



**RESOLUTION NO. 15-1(9)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of residential streets in an area northwest of Hanson Blvd and Main St, an area north of 121st Ave and east of Hanson Blvd, an area south of 121st Ave and east of the BNSF Railroad track, and an area south of Foley Blvd between Kumquat St and Holly Cir by street reconstruction, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement: (3 lowest)

North Valley, Inc.	\$1,625,486.65
Northwest Asphalt	\$1,667,011.00
Hardrives	\$1,692,418.38

**WHEREAS**, it appears that North Valley, Inc. of Nowthen, Minnesota is the lowest responsible bidder; and

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the bid of \$1,625,486.65 by North Valley, Inc. for Coon Rapids Improvement Project 15-1 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with North Valley, Inc. of Nowthen, Minnesota for the improvement of residential streets in an area northwest of Hanson Blvd and Main St, an area north of 121st Ave and east of Hanson Blvd, an area south of 121st Ave and east of the BNSF Railroad track, and an area south of Foley Blvd between Kumquat St and Holly Cir by street reconstruction according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk

**RESOLUTION NO. 15-1(12)**

**(12) RESOLUTION ADOPTING ASSESSMENT**

**WHEREAS**, pursuant to proper notice duly given as required by law, the Council has met and heard and passed upon all objections to the proposed assessment for the improvement of residential streets in an area northwest of Hanson Blvd, an area north of 121st Ave and east of Hanson Blvd, an area south of 121st Ave and east of the BNSF Railroad track, and an area south of Foley Blvd between Kumquat St and Holly Cir; and

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF COON RAPIDS, MINNESOTA:**

1. Such proposed assessment is hereby accepted and shall constitute the special assessment against the lands named therein, and each tract of land therein included is hereby found to be benefitted by the proposed improvement in the amount of the assessment levied against it.
2. Such assessment shall be payable in equal annual installments extending over a period of 10 years, beginning the first Monday in January, 2016 and shall bear interest at the rate of 3.04% per annum from the date of the adoption of this assessment Resolution.
3. The owner of any property so assessed may, at any time prior to certification of the assessment to the County Property Records and Taxation Division, pay the whole of the assessment on such property, with interest accrued to the date of payment, to the City Treasurer, except that no interest shall be charged if the entire assessment is paid within 30 days from the adoption of this Resolution; and the owner may, at any time thereafter, pay to the City Treasurer the entire amount of the assessment remaining unpaid. Such payment must be made before November 14 or interest will be charged through December 31 of the next succeeding year.
4. The Clerk shall forthwith transmit a certified duplicate of this assessment to the County Property Records and Taxation Division to be extended on the proper tax lists of the County, and such assessments shall be collected and paid over in the same manner as other municipal taxes.

Adopted this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk





## City Council Regular

12.

**Meeting Date:** 04/07/2015

**Subject:** Adopt Resolution No. 15-3(9) Awarding a Contract for Street Reconstruction - Project 15-3

**Submitted For:** Mark Hansen, Assistant City Engineer

**From:** Cher Ridout, Admin Secretary II

---

### **INTRODUCTION**

The City is proposing reconstruction of 2.6 miles of residential streets in an area north of Egret Boulevard and east of the BNSF Railroad tracks, an area north of Egret Boulevard and west of Highway 10, an area south of 101st Avenue and west of University Avenue, an area north of Coon Rapids Boulevard between 95th Avenue and Springbrook Drive, and a Frontage Road located north of Coon Rapids Boulevard between 111th Avenue and Crooked Lake Boulevard. Bids were received on April 3, 2015 for the improvement project. Council is requested to award a contract and adopt the assessment.

### **DISCUSSION**

The City is proposing to reconstruct approximately 2.6 miles of residential streets. Proposed improvements include reclaiming existing bituminous surface and aggregate base, removal and replacement of damaged curb and gutter, sidewalk and pedestrian curb ramp improvements in accordance with ADA requirements, replacement of watermain valves and hydrants, and repairs or replacements of the existing storm sewer as needed.

Two neighborhood meetings were held to inform residents on the project and answer any questions. The meetings were held on November 13, 2014 and January 29, 2015. City Council held a public hearing and assessment hearing for the project on February 17, 2015. Benefiting properties include single-family, multi-family, condominium/townhomes, and commercial properties. The proposed assessment rate is \$1,803.00 for single-family residential properties, \$22.53 per front foot for multi-family residential properties, and \$45.04 for commercial properties. The assessments would be levied over a 10-year period at an interest rate of 3.04%.

At the assessment hearing, Kurt Stachowski at 9424 Flintwood Street NW indicated he felt a reduced assessment rate is warranted for him and those that live on his block, since the City's plan at the time was not to add concrete curb and gutter to his street. After the hearing, staff reviewed the street in the field, and have since revised the plans to include the installation of concrete curb and gutter on Flintwood Street, as well as along 94th Avenue.

Bids for this project are scheduled to be opened at 10 am on Friday, April 3rd. Due to the timing of posting the City Council agenda, the final bid tabulation and recommendation for award will be forwarded to the Council on Friday afternoon, along with a project funding summary. A corresponding contract award resolution (15-3(9)) will then be prepared and submitted for approval on the April 7th regular Council meeting agenda.

### **RECOMMENDATION**

It is recommended the Council take the following action:

- a. Adopt Resolution No. 15-3(9) awarding a contract to \_\_\_\_ in the total project cost of \$ \_\_\_\_.
- b. Adopt Resolution No. 15-3(12) adopting the assessment.

---

**BUDGET IMPACT:**

The construction cost for this project is \$\_\_\_\_. Adding 20% to the construction costs for contingency and engineering services would bring the total estimated project cost to \$\_\_\_\_. A total of \$538,144.70 is proposed to be assessed to benefiting properties. The balance of the project costs would be recovered from various funds. The amount of \$\_\_\_\_ would be recovered from the Street Reconstruction Fund (797), \$\_\_\_\_ would come from the Sanitary Sewer Fund (620), \$\_\_\_\_ would come from the Storm Water Drainage Fund (640) recovered through storm drainage charges, and \$\_\_\_\_ would come from the Water System Maintenance Fund (601) recovered through charges for water used.

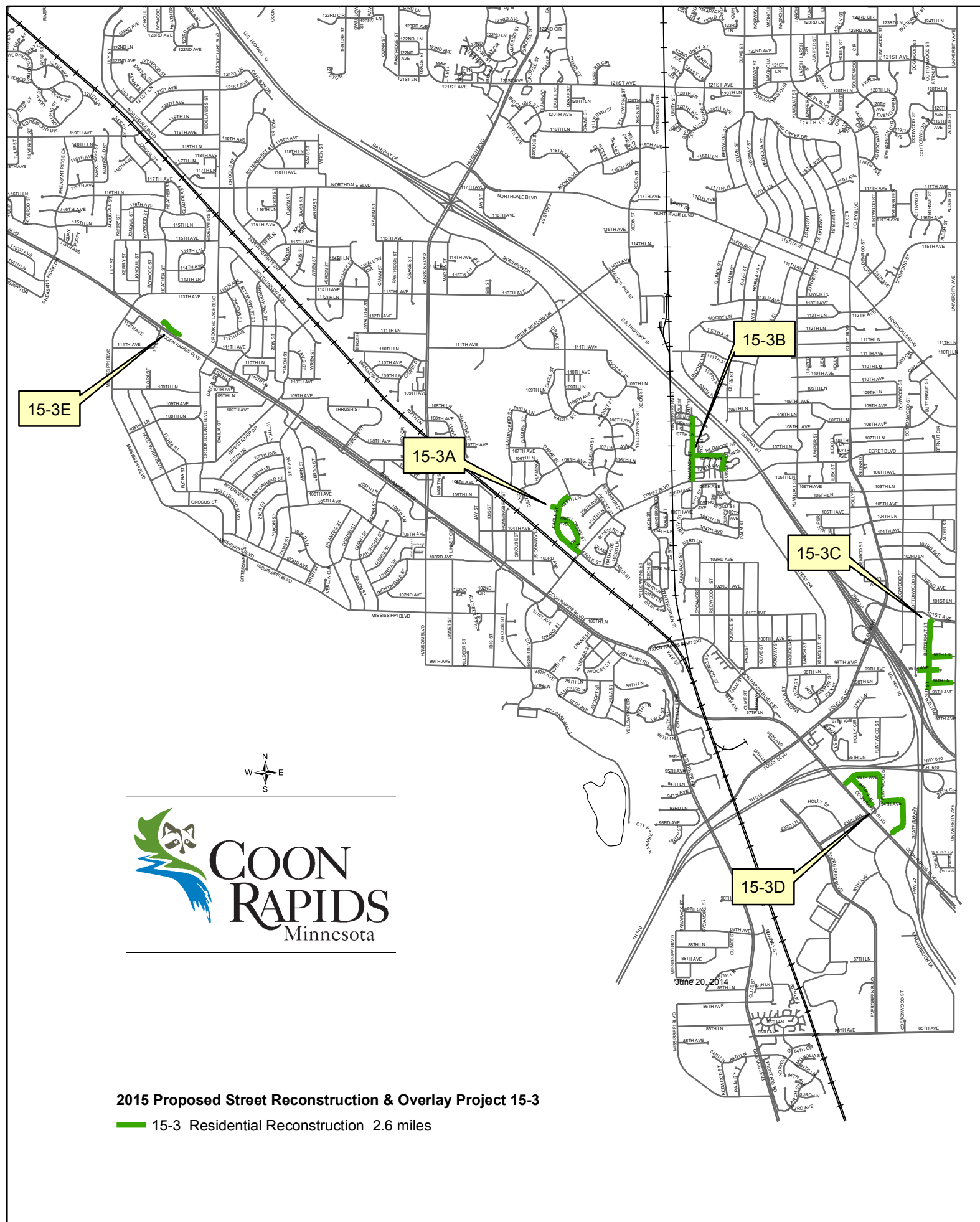
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**Attachments**

Location Map

Resolution 15-3(12)

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**RESOLUTION NO. 15-3(12)**

**(12) RESOLUTION ADOPTING ASSESSMENT**

**WHEREAS**, pursuant to proper notice duly given as required by law, the Council has met and heard and passed upon all objections to the proposed assessment for the improvement of residential streets in an area north of Egret Blvd and east of the BNSF Railroad tracks, an area north of Egret Blvd and west of Highway 10, an area south of 101st Ave and west of University Ave, an area north of Coon Rapids Blvd between 95th Ave and Springbrook Dr, and a Frontage Road located north of Coon Rapids Blvd between 111th Ave and Crooked Lake Blvd by street reconstruction; and

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF COON RAPIDS, MINNESOTA:**

1. Such proposed assessment is hereby accepted and shall constitute the special assessment against the lands named therein, and each tract of land therein included is hereby found to be benefitted by the proposed improvement in the amount of the assessment levied against it.
2. Such assessment shall be payable in equal annual installments extending over a period of 10 years, beginning the first Monday in January, 2016 and shall bear interest at the rate of 3.04% per annum from the date of the adoption of this assessment Resolution.
3. The owner of any property so assessed may, at any time prior to certification of the assessment to the County Property Records and Taxation Division, pay the whole of the assessment on such property, with interest accrued to the date of payment, to the City Treasurer, except that no interest shall be charged if the entire assessment is paid within 30 days from the adoption of this Resolution; and the owner may, at any time thereafter, pay to the City Treasurer the entire amount of the assessment remaining unpaid. Such payment must be made before November 14 or interest will be charged through December 31 of the next succeeding year.
4. The Clerk shall forthwith transmit a certified duplicate of this assessment to the County Property Records and Taxation Division to be extended on the proper tax lists of the County, and such assessments shall be collected and paid over in the same manner as other municipal taxes.

Adopted this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

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Joan Lenzmeier, City Clerk



## City Council Regular

13.

**Meeting Date:** 04/07/2015

**Subject:** Contract Award for 2015 Street Maintenance Program, Project 15-5

**Submitted For:** Tim Himmer, Public Works Director **From:** Cher Ridout, Admin Secretary II

### **INTRODUCTION**

On March 13, 2015, bids were received for various materials and services to be provided under the Regional Street Maintenance Joint Powers Agreement. Items included in this bid were sealcoating, pavement markings, street sweeping, crack sealing and fog sealing. Cities participating in the 2015 program include Andover, Brooklyn Center, Columbia Heights, Coon Rapids, East Bethel, Fridley, Ham Lake, and Mahtomedi. Council is requested to award contracts for these services.

### **DISCUSSION**

On February 17, 2015, Council approved plans and specifications and ordered advertisement for bids. In accordance with Council direction, bids were received for various street maintenance materials and services on March 13, 2015. We received three bids for sealcoating, three bids for pavement markings, two bids for street sweeping, four bids for crack sealing, and two bids for fog sealing. A summary of bids and a bid tabulation showing unit prices is attached. The following is an analysis for each item of the program:

#### **Sealcoating**

Sealcoating was bid on a volume basis for oil and on an area basis for rock. The base bid includes regular asphalt emulsion sealcoat oil and trap rock cover aggregate. An alternate bid was requested from the contractor to provide granite. The low bid was received from Allied Blacktop Company. Allied Blacktop Company has provided sealcoating for the City for many years. Bids received are as follows:

Contractor	Base Bid	Alternate
Allied Blacktop Company	\$1,409,830.84	\$1,407,934.35
Pearson Bros.	\$1,457,438.49	\$1,442,266.57
Asphalt Surface Technologies	\$1,526,232.15	\$1,472,100.18

Again this year, we will be using a polymer additive to sealcoat oil and a different type of cover aggregate in high traffic areas, including cul-de-sacs and higher volume collector streets. Previous experience has shown this type of application has minimized scuffing and bleeding of sealcoat material.

#### **Pavement Markings**

Pavement markings were bid by the gallon for street markings, parking lot markings, and street symbols. The low bid for pavement markings was submitted by AAA Striping Service, Co. in the amount of \$197,720.60. AAA Striping Service, Co. has completed several pavement markings contracts in the past for the City. Bids received are as follows:

Contractor	Bid
AAA Striping Service, Co.	\$197,720.60
Traffic Marking Service, Inc.	\$224,345.92
Warning Lites of Minnesota, Inc.	\$237,629.84

### **Street Sweeping**

Street sweeping was bid on an hourly rental basis. The low bid for seasonal sweeping was submitted by Pearson Bros. in the amount of \$61,800.00. Bid prices for street sweeping have not increased from last year. Pearson Bros. has been doing the street sweeping for the City for several years. Bids received are as follows:

Contractor	Spring Sweeping	Fall Sweeping	Total Bid
Pearson Bros.	\$30,900.00	\$30,900.00	\$61,800.00
Allied Blacktop Company	\$35,432.00	\$35,432.00	\$70,864.00

### **Crack Sealing**

Crack sealing was bid on a lineal foot basis. The low bid for crack sealing was submitted by Allied Blacktop Company in the amount of \$293,623.86.00. Their unit price bid is comparable to 2014. Bids received are as follows:

Contractor	Bid
Allied Blacktop Company	\$293,623.86
Asphalt Surface Technologies	\$341,115.74
American Pavement Solutions, Inc.	\$354,356.40
Asphalt Surface Technologies Corporation	\$428,938.46

The Joint Powers Agreement stipulates that each city provide written concurrence for the award of bids, 20 days after notice of receipt of the bids. We have received concurrence from all participating cities as required. As outlined in the JPA, the cities will now have 30 days to decide whether or not they wish to proceed. Sealcoating and crack sealing will begin in early June and be completed in September. Pavement markings and street sweeping services may continue later in the year as weather permits.

### **RECOMMENDATION**

It is recommended the City Council approve the following resolutions:

- a. Adopt Resolution No. 15-5(9A) awarding a contract to Allied Blacktop Company for bituminous sealcoating in the base bid amount of \$1,409,830.84.
- b. Adopt Resolution No. 15-5(9B) awarding a contract to AAA Striping Service Co. for pavement markings in the amount of \$197,720.60.
- c. Adopt Resolution No. 15-5(9C) awarding a contract to Pearson Bros. for seasonal street sweeping in the amount of \$61,800.00.
- d. Adopt Resolution No. 15-5(9D) awarding a contract to Allied Blacktop Company for crack sealing in the amount of \$293,623.86.

e. Adopt Resolution No. 15-5(9E) awarding a contract to Allied Blacktop Company for fog sealing in the amount of \$44,079.10,

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**Attachments**

Summary of Bids

Bid Tabulation

Resolution No. 15-5(9A)

Resolution No. 15-5(9B)

Resolution No. 15-5(9C)

Resolution No. 15-5(9D)

Resolution No. 15-5(9E)

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## 2015 STREET MAINTENANCE MATERIALS SUMMARY OF BIDS

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<u>Sealcoating</u>	<u>Base Bid</u>	<u>Alternate</u>
Allied Blacktop Company	\$1,409,830.84	\$1,407,934.35
Pearson Bros.	\$1,457,438.49	\$1,442,266.57
Asphalt Surface Technologies	\$1,526,232.15	\$1,472,100.18

### Pavement Markings

AAA Striping Service	\$197,720.60
Traffic Marking Service	\$224,345.92
Warning Lites of MN	\$237,629.84

### Street Sweeping

Pearson Bros.	\$61,800.00
Allied Blacktop Company	\$70,864.00

### Crack Sealing

Allied Blacktop Company	\$293,623.86
Asphalt Surface Technologies	\$341,115.74
American Pavement	\$354,356.40
Fahrner Asphalt Sealers	\$428,938.46

### Fog Sealing

Allied Blacktop Company	\$44,079.10
Pearson Bros.	\$61,818.25



**BID OPENING: MARCH 13, 2015, 10:00 A.M.**

NO.	DESCRIPTION	UNIT	QTY	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL	UNIT PRICE	TOTAL
	BID SCHEDULE NO. 1 - SEAL COAT																		
1	AGGREGATE																		
	DRESSER TRAP ROCK (BASE BID)																		
	F&I FA-2 AGGREGATE	SY	189649	0.93	176,373.57	0.60	113,789.40	0.78	147,926.22										
	F&I FA-2 (MODIFIED 1/8")	SY	779201	0.985	767,512.99	0.74	576,608.74	0.93	724,656.93										
	TOTAL DRESSER TRAP ROCK				943,886.56		690,398.14		872,583.15										
	GRANITE (ALTERNATE)																		
	F&I FA-2 AGGREGATE	SY	189649	0.92	174,477.08	0.52	98,617.48	0.70	132,754.30										
	F&I FA-2 (MODIFIED 1/8")	SY	779201	0.985	767,512.99	0.74	576,608.74	0.88	685,696.88										
	TOTAL GRANITE				941,990.07		675,226.22		818,451.18										
2	SEAL COAT OIL																		
	F&I CRS-2 SEAL COAT OIL	GAL	158635	1.60	253,816.00	2.75	436,246.25	2.30	364,860.50										
	F&I CRS-2P (POLYMER MOD)	GAL	105014	2.02	212,128.28	3.15	330,794.10	2.75	288,788.50										
	TOTAL SEAL COAT OIL				465,944.28		767,040.35		653,649.00										
	TOTAL BID SCHEDULE NO. 1 (BASE BID)				1,409,830.84		1,457,438.49		1,526,232.15										
	TOTAL BID SCHEDULE NO. 1 (ALTERNATE)				1,407,934.35		1,442,266.57		1,472,100.18										
	BID SCHEDULE NO. 2 - PAVEMENT MARKINGS																		
1	F&I STREET MARKINGS	GAL	6364					19.15	121,870.60	21.28	135,425.92	24.06	153,117.84						
2	F&I PARKING LOT MARKINGS	GAL	30					95.00	2,850.00	117.00	3,510.00	111.20	3,336.00						
3	F&I STREET SYMBOLS	GAL	730					100.00	73,000.00	117.00	85,410.00	111.20	81,176.00						
	TOTAL BID SCHEDULE NO. 2								197,720.60		224,345.92		237,629.84						
	BID SCHEDULE NO. 3 - STREET SWEEPING																		
1	PROVIDE SPRING SWEEPING	HR	412	86.00	35,432.00	75.00	30,900.00												
2	PROVIDE FALL SWEEPING	HR	412	86.00	35,432.00	75.00	30,900.00												
	TOTAL BID SCHEDULE NO. 3				70,864.00		61,800.00												
	BID SCHEDULE NO. 4 - CRACK SEALING																		
1	F&I STREET ROUTE & SEAL CRACK SEALING	LF	161439	0.70	113,007.30			0.77	124,308.03							0.76	122,693.64	0.89	143,680.71
2	F&I STREET BLOW & GO CRACK SEALING	LF	373804	0.465	173,818.86			0.56	209,330.24							0.60	224,282.40	0.74	276,614.96
3	F&I TRAIL ROUTE & SEAL CRACK SEALING	LF	9711	0.70	6,797.70			0.77	7,477.47							0.76	7,380.36	0.89	8,642.79
4	F&I TRAIL BLOW & GO CRACK SEALING	LF	0	0.465				0.56								0.60		0.74	
	TOTAL BID SCHEDULE NO. 4				293,623.86				341,115.74								354,356.40		428,938.46
	BID SCHEDULE NO. 5 - FOG SEALING																		
1	F&I STREET FOG SEALING	GAL	0	7.50															
2	F&I TRAIL FOG SEALING	GAL	10751	4.10	44,079.10	5.75	61,818.25												
	TOTAL BID SCHEDULE NO. 5				44,079.10		61,818.25												
	SUMMARY OF BIDS																		
	TOTAL SEALCOATING (BASE BID)				1,409,830.84		1,457,438.49		1,526,232.15										
	TOTAL SEALCOATING (ALTERNATE)				1,407,934.35		1,442,266.57		1,472,100.18										
	TOTAL PAVEMENT MARKINGS									197,720.60		224,345.92		237,629.84					
	TOTAL STREET SWEEPING				70,864.00		61,800.00												
	TOTAL CRACK SEALING				293,623.86				341,115.74								354,356.40		428,938.46
	TOTAL FOG SEALING				44,079.10		61,818.25												

**RESOLUTION NO. 15-5(9A)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of city streets in the cities of Andover, Brooklyn Center, Columbia Heights, Coon Rapids, East Bethel, Fridley, and Mahtomedi by bituminous sealcoating, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement:

<u>Contractor</u>	<u>Base Bid</u>	<u>Alternate Bid</u>
Allied Blacktop Company	\$1,409,830.84	\$1,407,934.35
Pearson Bros. Inc.	\$1,457,438.49	\$1,442,266.57
Asphalt Surface Technologies	\$1,526,232.15	\$1,472,100.18

**WHEREAS**, it appears that Allied Blacktop Company of Maple Grove, Minnesota is the lowest responsible bidder; and

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the base bid of \$1,409,830.84 and alternate bid of \$1,407,934.35 by Allied Blacktop Company for Coon Rapids Improvement Project 15-5 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with Allied Blacktop Company of Maple Grove, Minnesota for the improvement of city streets by bituminous sealcoating according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

\_\_\_\_\_  
Jerry Koch, Mayor

ATTEST:

\_\_\_\_\_  
Joan Lenzmeier, City Clerk

**RESOLUTION NO. 15-5(9B)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of city streets in the cities of Andover, Columbia Heights, Coon Rapids, East Bethel, Fridley, Ham Lake, and Mahtomedi by pavement markings, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement:

<u>Contractor</u>	<u>Bid</u>
AAA Striping Service, Co.	\$197,720.60
Traffic Marking Service, Inc.	\$224,345.92
Warning Lites of Minnesota, Inc.	\$237,629.84

**WHEREAS**, it appears that AAA Striping Service, Co. of St. Michael, Minnesota is the lowest responsible bidder,

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the bid of \$197,720.60 by AAA Striping Service Co. for Coon Rapids Improvement Project 15-5 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with AAA Striping Service Co. of St. Michael, Minnesota for the improvement of city streets by pavement markings according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

\_\_\_\_\_  
Jerry Koch, Mayor

ATTEST:

\_\_\_\_\_  
Joan Lenzmeier, City Clerk

**RESOLUTION NO. 15-5(9C)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of city streets in the cities of Coon Rapids, Fridley, Ham Lake and Mahtomedi by street sweeping, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement:

<u>Contractor</u>	<u>Bid</u>
Pearson Bros.	\$61,800.00
Allied Blacktop Company	\$70,864.00

**WHEREAS**, it appears that Pearson Bros. of Hanover, Minnesota is the lowest responsible bidder,

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the bid of \$61,800.00 by Pearson Bros. for Coon Rapids Improvement Project 15-5 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with Pearson Bros. of Hanover, Minnesota for the improvement of city streets by street sweeping according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

\_\_\_\_\_  
Jerry Koch, Mayor

ATTEST:

\_\_\_\_\_  
Joan Lenzmeier City Clerk

**RESOLUTION NO. 15-5(9D)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of city streets in the cities of Andover, Brooklyn Center, Columbia Heights, East Bethel, Fridley, Ham Lake, and Mahtomedi by crack sealing, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement:

<u>Contractor</u>	<u>Bid</u>
Allied Blacktop Company	\$293,623.86
Asphalt Surface Technologies	\$341,115.74
American Pavement Solutions, Inc.	\$354,356.40
Fahrner Asphalt Sealers	\$428,938.46

**WHEREAS**, it appears that Allied Blacktop Company of Maple Grove, Minnesota is the lowest responsible bidder,

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the bid of \$293,623.86 from Allied Blacktop Company for Coon Rapids Improvement Project 15-5 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with Allied Blacktop Company of Maple Grove, Minnesota for the improvement of city streets by crack sealing according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

\_\_\_\_\_  
Jerry Koch, Mayor

ATTEST:

\_\_\_\_\_  
Joan Lenzmeier, City Clerk

**RESOLUTION NO. 15-5(9E)**

**(9) RESOLUTION ACCEPTING BID AND  
AWARDING CONTRACT**

**WHEREAS**, pursuant to an advertisement for bids for the improvement of city streets in the cities of Andover and Mahtomedi by fog sealing, bids were received, opened and tabulated according to law, and the following bids were received complying with the advertisement:

<u>Contractor</u>	<u>Bid</u>
Allied Blacktop Company	\$44,079.10
Pearson Bros. Inc.	\$61,818.25

**WHEREAS**, it appears that Allied Blacktop Company of Maple Grove, Minnesota is the lowest responsible bidder,

**WHEREAS**, the City of Coon Rapids expects to reimburse all or a portion of the project expenditures with the proceeds of debt to be incurred by the City; and

**WHEREAS**, this declaration is made pursuant to Section 1.103-18 of the Income Tax Regulations of the Internal Revenue Service.

**NOW, THEREFORE, BE IT RESOLVED** by the City Council of Coon Rapids, Minnesota that the bid of \$44,079.10 from Allied Blacktop Company for Coon Rapids Improvement Project 15-5 be accepted as the lowest responsible bid.

**BE IT FURTHER RESOLVED** that the Mayor and City Manager are hereby authorized and directed to enter into a contract with Allied Blacktop Company of Maple Grove, Minnesota for the improvement of city streets by fog sealing according to the plans and specifications therefore approved by the City Council and on file in the office of the City Clerk.

**BE IT FURTHER RESOLVED** that the City Clerk is hereby authorized and directed to return forthwith to all bidders the deposits made with their bids, except that the deposits of the successful bidder and the next two lowest bidders shall be retained until a contract has been signed.

Adopted this 7th day of April, 2015.

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Jerry Koch, Mayor

ATTEST:

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Joan Lenzmeier, City Clerk



## City Council Regular

14.

**Meeting Date:** 04/07/2015

**Subject:** PC 15-12 , Consider Approval of an Amendment to Gateway Commerce Center Planned Unit Development

**From:** Scott Harlicker, Planner

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### **INTRODUCTION**

The applicant seeks approval to amend the final PUD to change the uses allowed to include an auto dealership and a swim center. The cul de sac will also be shortened and one of the restaurant sites will be eliminated. Allow flexibility as to where uses can locate within the development. Allow a defined mix of uses but do not tie them to a specific lot.

### **DISCUSSION**

#### **Background**

In October and November last year the applicant came before the Commission with a request to amend the PUD to allow a restaurant with a drive thru on a site that was identified in the PUD as a bank. Both the Commission and Council expressed concern about the incremental changes that have been occurring in the PUD. There had been two previous amendments to change uses on specific lots. The Commission thought that, given the changes that had occurred in the market, a more appropriate method to address those changes would be to request an amendment to the entire PUD. This would eliminate the need for additional incremental changes. The applicant was not in a position at that time to move forward with a proposal for overall changes to the PUD. Since those meetings, the applicant has two potential users for property at the end of the cul de sac, a car dealership and a swimcenter. With these potential users, the applicant would like to move forward with a comprehensive PUD amendment to address development of the undeveloped parcels.

#### **Analysis**

The applicant is proposing the following changes to the PUD:

- Allow a new car dealership and a swim club at the end of the cul de sac.
- Shorten the cul de sac to accommodate these new users.
- Reduce the number of lots along the east side of the cul de sac from five to four to accommodate the new users.
- Allow flexibility as to where uses can locate within the development. Allow a defined mix of uses but do not tie them to a specific lot.

#### *New Uses - New Car Dealership and Swim Center*

The uses currently identified for the lots at the end of the cul de sac include retail and office. The applicant is proposing to locate a new auto dealership and a swim center at that those locations.

The car dealership is not specifically identified in the PUD design guidelines as an allowed use. However, the design guidelines does include language that calls for a mix of commercial uses that are diverse yet complimentary. Large scale commercial uses were identified for the area along the Highway 10 entrance ramp. The proposed

dealership would fit both of those criteria. With the high visibility from Highway 10, the dealership would draw traffic into the development and past the other businesses. Given the proposed location, the new car dealership is an appropriate use in the PUD. However, to preclude this area from having the appearance of an "auto mall", it is recommended that the number of dealerships be limited to one.

The swim center is the other new use proposed for the PUD. However, unlike the car dealership the design guidelines specifically identified recreational uses as a preferred use in the PUD. The swim center would be classified as a recreational use. Given the approximate size of the facility (30,000sf) the proposed location along Highway 10 is appropriate. This use is also very complementary with the other uses in the PUD. People visiting the swim center for events can also stay at the proposed hotel, eat in the restaurants and buy merchandise in the retail businesses.

### *Changes in the Design*

To accommodate the auto dealership and the swim center, the applicant is modifying the layout of the PUD. The cul de sac is being shortened to increase the size of Lot 1 and make it more conducive for development. The cul de sac currently extends to the west end of the PUD. It is being shortened by about 600 feet, and the size of Lot 1 is increased from 2.9 acres to 6 acres. The existing Lots 2 and 3 are being combined into Lot 2 to provide the site for the swim center. Lot 4, which was the site for a restaurant, has been eliminated. The number of uses along the east side of the cul de sac has been reduced to four from five.

The building for the proposed car dealership is located in about the same location, relative to the street, as the current location for the retail building. However, the proposed swim center is located much closer to the street than the current location of the retail building. The current site plan has the retail building about 50 feet from the property line; the proposed location for the swim center is about 10 feet from the property line and 20 feet from the street. The proposed location of the swim center is acceptable provided additional landscaping is provided between the building and the street and the facade of the building is broken up by architectural features.

A sidewalk should be shown along the north side of the street from the end of the cul de sac to the crosswalk between Lots 5 and 6. The original PUD included a trail along wetlands and stormwater ponds on the north side of the PUD with connections to the street at the end of the cul de sac and near the hotel. This trail and sidewalk connections should also be shown.

The landscaping in the car dealership parking lot should be increased to make up for the trees that will not be installed because of the shortened cul de sac.

### *Use Location Flexibility*

Instead of tying a proposed use to a specific lot, the applicant is requesting flexibility as to which lot the uses can locate. With the current PUD, each lot has a specific use; the applicant is requesting that the uses be allowed to locate on any available vacant lot.

### Planning Commission Meeting

At the Planning Commission meeting held on March 19th, one resident spoke at the public hearing. The resident was concerned about the environmental impacts of a car dealership and the traffic it would generate. Stormwater runoff will be handled by the stormwater ponds constructed by the applicant. No additional pollution will result from the car dealership parking lot than would occur in a typical parking lot for a retail business. All servicing of cars will take place inside the building limiting potential off site contamination. The car dealership will generate less traffic than a retail use. The signalized intersection was designed to accommodate full build out of this PUD.

The Commission discussed signage. Each site is allowed a 10 foot tall monument sign. The Commission sought clarification regarding landscaping. Additional landscaping will be required in front of the dealership equal to the landscaping required in the original plan. The Commission also asked about stormwater management. Stormwater ponds were constructed as part of the overall grading of the site. However, any new development will have to apply for all applicable permits, including those required by Coon Creek Watershed District.



The Commission voted unanimously to recommend approval of the proposed PUD amendment.

### **RECOMMENDATION**

In Planning Case 15-12, the Commission recommended the City Council approve the amendment to the PUD with the following conditions:

1. Only one primarily new car dealership is allowed in the PUD
2. All conditions of the original PUD approval of December 18, 2007 are included in this approval
3. The applicant receive all permits necessary for the grading done as part of the cu de sac work.
4. Approval and execution of an amendment to the PUD agreement.
5. The sidewalk along Gateway Drive must be shown on the site plan.
6. Additional trees must be installed in the dealership parking lot and between the parking lot and the property line so that it is consistent the previously approved PUD.
7. The sidewalk connections between the trail along the wetlands and Gateway Drive must be shown.
8. Additional landscaping must be installed between the swim center and Gateway Drive.
9. The elevation of the swim center building facing Gateway Drive must include varying walls, planes or other architectural features to provide articulation to the building.
10. Applicant must receive all necessary permits from Coon Creek Watershed District.

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### **Attachments**

Location Map

Applicant's Narrative

Current PUD Site Plan

Proposed PUD Site Plan

Current Landscape Plan

Proposed Landscape Plan

PUD Design Guidelines

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# Location Map







February 10, 2015

***Re: PUD Amendment at the Gateway Commerce Center***

Dear Coon Rapids City Council and Planning Commission:

This letter shall serve as a <sup>d</sup> request to amend the remaining undeveloped lots of the PUD at the Coon Rapids Gateway Commerce Center.

Last October, The Gateway Commerce Center requested a PUD Amendment for Lot 9 changing its use from a Bank to a Drive-Thru Restaurant. At that time it was suggested that H&W Family, LLLP should take a hard look at the current PUD and perhaps come in with a new PUD Amendment that allows the flexibility necessary to finish developing the parcels left which becomes an asset to both the City of Coon Rapids and its residents.

This letter is to request a revision of the remaining lots in the PUD which are: Lot 1, 2, 3, 4, 5, 6, 7, 10, and 13.

In 2006-2007 we spent over 1 year working with the city staff, planning commission, architectural city consultants, and the city council in order to produce a PUD that worked for both the City of Coon Rapids and H&W Family, LLLP. This process allowed for design guidelines to be created which governed the PUD and the site development. It was the intent of the PUD to allow for restaurants, drive-thru's, retail, office, gas, daycare, and medical. Since that time we have seen a real estate depression, economy recession, and a major change in the way retailers are now relying on internet sales. Therefore it is time to take a new direction with the Gateway Commerce Center which will add uses that are both attractive and market ready.

H&W Family, LLLP has worked very hard to make sure that all the current retail/gas/restaurant buildings were built at a high quality level which is an asset to the residents of Coon Rapids. Caribou, Holiday, and McDonalds are all extremely successful at this intersection and are exceeding sales expectations. This shows that the residents of Coon Rapids are extremely happy with these uses and this development thus far. Our intent is to go on having Grade A buildings and uses that meet the market demands.

Exhibit A, (below), illustrates a new PUD with some similar and new uses, similar use locations with flexibility, and the same design guidelines for quality that we have always had. As a developer, the hardest part of doing a commercial development is predicting the future. We tried that approach in the past with some success and some failure. Our new approach is the guide the uses and locations that are current with today's and we request location flexibility in order to attract the hotel, retail, medical, and restaurant users.

In the past we tied a use to a certain location. We have realized that is very challenging as all users like to have a say in their development location. Our request is to allow flexibility on the lot uses. An example is allowing the Hotel to be located in the development on several lots rather than one defined location. This way we can develop the land per the users request and bring you a use that the residents will enjoy.

In order to continue the success of the development, we are requesting one new use for Lot 1 and 2 located along Highway 10. A very well-known high quality business that is already location in Coon Rapids has requested a 6 acre site in this development. The Walser Group is looking to add another location in Coon Rapids which will serve as a New Car Dealership. Walser's name represents quality and their intension is to bring another high quality building and dealership to Coon Rapids. Gone are the old days of car dealerships and in its place has arrived car dealerships with Quality Buildings, Quality Products, and Quality Service.

The Walser building will be designed to incorporate elements of the design guidelines to continue the look and feel of the current development. You will see material choices and blend in with the other users in the development and their business will help support the addition of restaurants, Swim Clubs, Hotels, and others. The Walser dealership will bring new high tech jobs and sales jobs to Coon Rapids. It will draw customers



from all over the state of Minnesota which serves a positive impact for uses like restaurants and hotels.

Another exciting use we are currently working with is a Swim Event Center that will feature Swimming Meets, Swimming Lessons, and Swimming Therapy classes. This Event Center is sponsored by the local swim club chapter named The Great Wolf Swim Club. The club is already hosting events and teaching children to swim in the local communities around Anoka County. David Benz, the clubs director, has stated Anoka County's residents are in full support of this club which gives their children the opportunity to compete at the national level. This Swim Center would be like no other in the State of Minnesota and they will attract local swimmers, swimmers from the entire state, and swimmers from the Mid-West Region. The Swim Center will feature a competition pool, swimming lesson pool, and a therapy pool like none other. This facilities local and regional draw will again positively promote a hotel, restaurants, retail, and medical to the development.

H&W Family, LLLP's intent with this PUD Revision is to promote market uses, attract new retail, medical, restaurant, and hotel users to Coon Rapids. We have the ability to continue to make this development a huge success with your help. Our request is to allow for current lot uses, new lot uses, and to have more flexibility with use locations in order to deliver the City of Coon Rapids a High Quality Development that will continue to be successful for the long term.

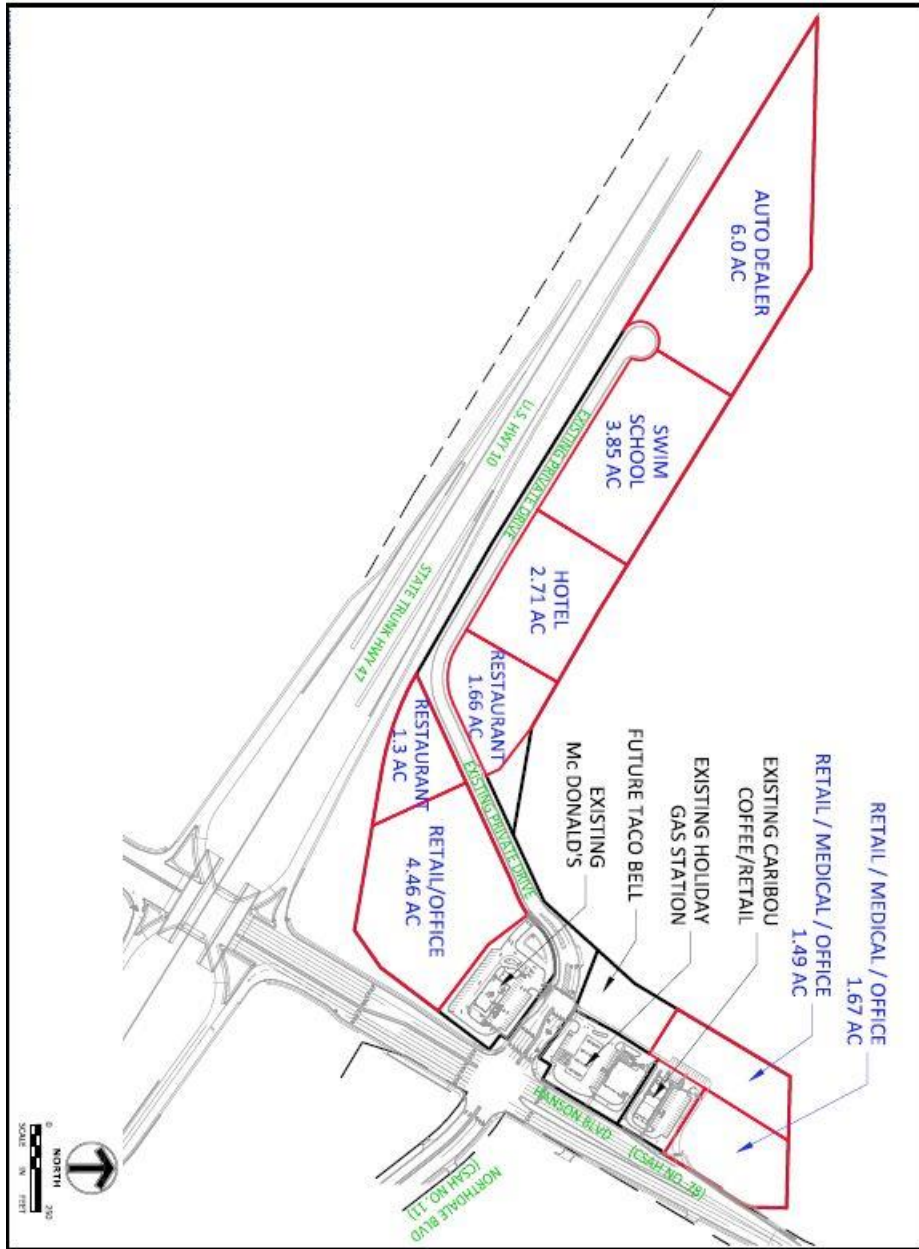
Please feel free to contact the development team with any questions and thanks for your support.

Best Regards,

Jonathan Adam  
Silverstone Realty  
7955 Stone Creek Dr. Unit 130  
Chanhassen MN 55317  
(952) 856-6231  
[Jadam@silverstonemn.com](mailto:Jadam@silverstonemn.com)

On Behalf of H&W Family, LLLP

Exhibit A:



## Exhibit B: PUD Change Requests:

1. Allow Restaurant, Retail, Hotel, Medical, and Office uses on any of the remaining lots in the PUD where feasible with the same set of Design Guideline Standards which would not require a PUD Amendment Modification. (ex: A Hotel may be placed on Lot 3/4/6/7)
2. Allow for a New Use on Lot 1 and Lot 2 of an Auto Dealership.
3. Allow for a location modification to the existing cul-de-sac drive which moves it to the South East allowing for the use of developer corrected materials.
4. Allow for the same amount of sign locations along Highway 10 with a location modification.

Exhibit C: Auto Dealership Quality Building Example:

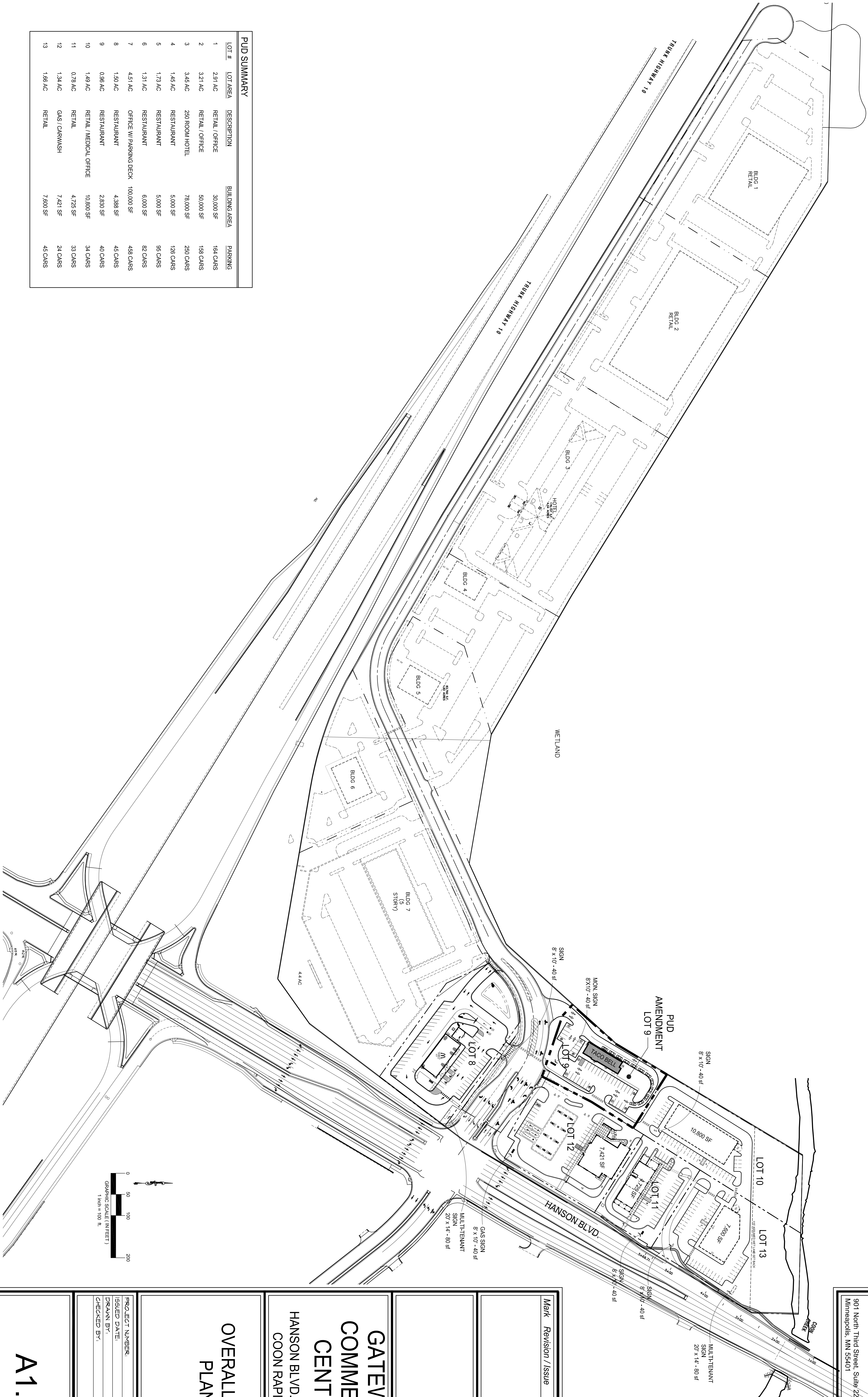




Exhibit D: Swim Club Pool Interior







PUD SUMMARY			
LOT #	LOT AREA	DESCRIPTION	BUILDING AREA      PARKING
1	2.91 AC	RETAIL / OFFICE	30,000 SF      164 CARS
2	3.21 AC	RETAIL / OFFICE	50,000 SF      168 CARS
3	3.45 AC	260 ROOM HOTEL	78,000 SF      290 CARS
4	1.45 AC	RESTAURANT	5,000 SF      128 CARS
5	1.73 AC	RESTAURANT	5,000 SF      95 CARS
6	1.31 AC	RESTAURANT	6,000 SF      82 CARS
7	4.51 AC	OFFICE W/ PARKING DECK	100,000 SF      458 CARS
8	1.50 AC	RESTAURANT	4,388 SF      45 CARS
9	0.96 AC	RESTAURANT	2,890 SF      40 CARS
10	1.49 AC	RETAIL / MEDICAL OFFICE	10,800 SF      34 CARS
11	0.78 AC	RETAIL	4,725 SF      33 CARS
12	1.34 AC	GAS / CARWASH	7,421 SF      24 CARS
13	1.66 AC	RETAIL	7,600 SF      45 CARS

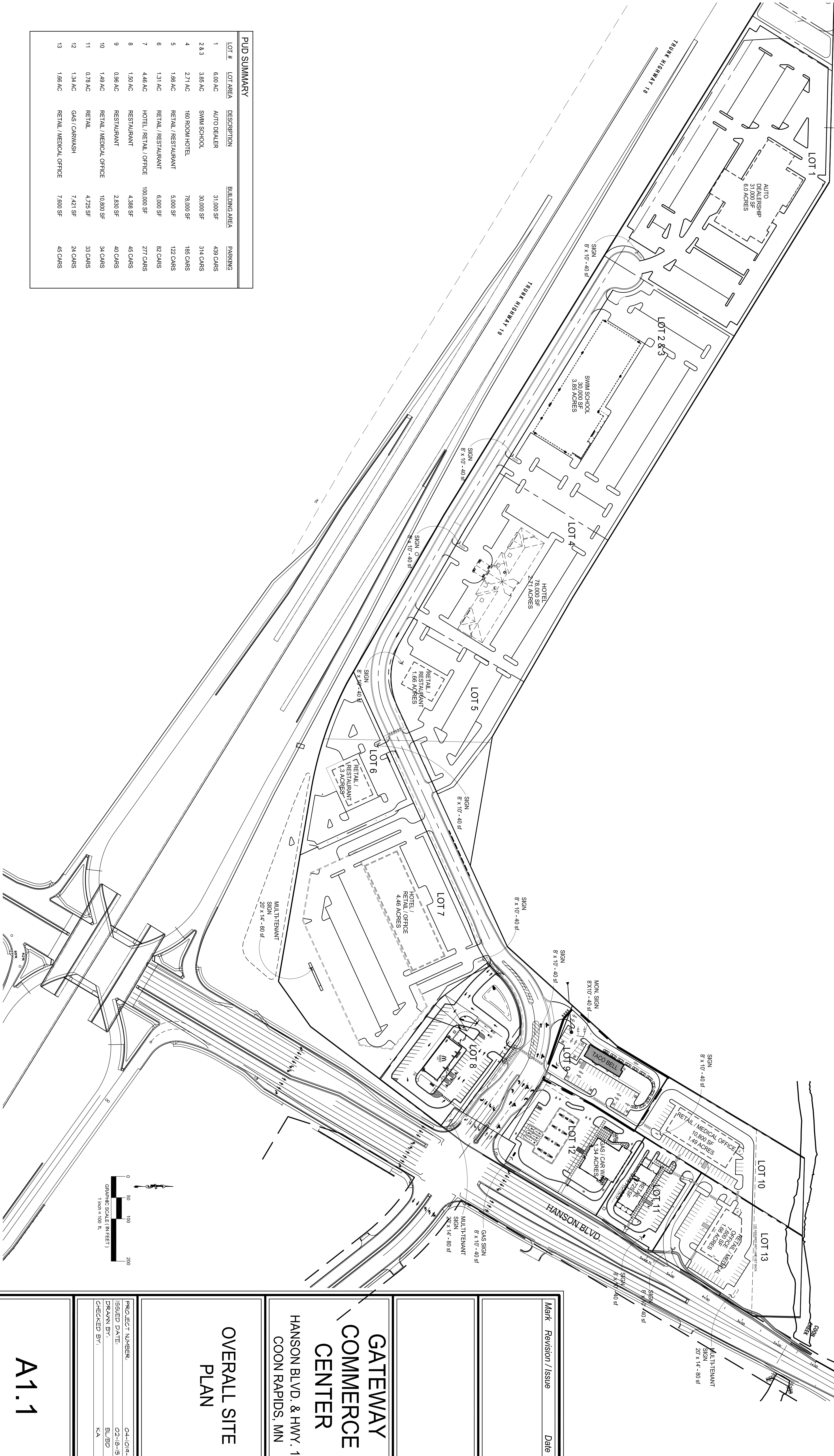
Mark      Revision / Issue      Date

GATEWAY  
COMMERCE  
CENTER  
  
HANSON BLVD. & HWY. 10  
COON RAPIDS, MN

OVERALL SITE  
PLAN

PROJECT NUMBER: 04-1014-C1  
ISSUED DATE: 04-08-14  
DRAWN BY: BL/BD  
CHECKED BY: KA

A1.1



PUD SUMMARY			
LOT #	LOT AREA	DESCRIPTION	BUILDING AREA
1	6.00 AC	AUTO DEALER	31,000 SF
2 & 3	3.85 AC	SWIM SCHOOL	3,350 SF
4	2.71 AC	160 ROOM HOTEL	78,000 SF
5	1.66 AC	RETAIL / RESTAURANT	5,000 SF
6	1.31 AC	RETAIL / RESTAURANT	6,000 SF
7	4.46 AC	HOTEL / RETAIL / OFFICE	100,000 SF
8	1.50 AC	RESTAURANT	4,388 SF
9	0.96 AC	RESTAURANT	2,830 SF
10	1.49 AC	RETAIL / MEDICAL OFFICE	10,800 SF
11	0.78 AC	RETAIL	4,725 SF
12	1.34 AC	GAS / CARWASH	7,421 SF
13	1.66 AC	RETAIL / MEDICAL OFFICE	7,600 SF

Mark Revision / Issue Date

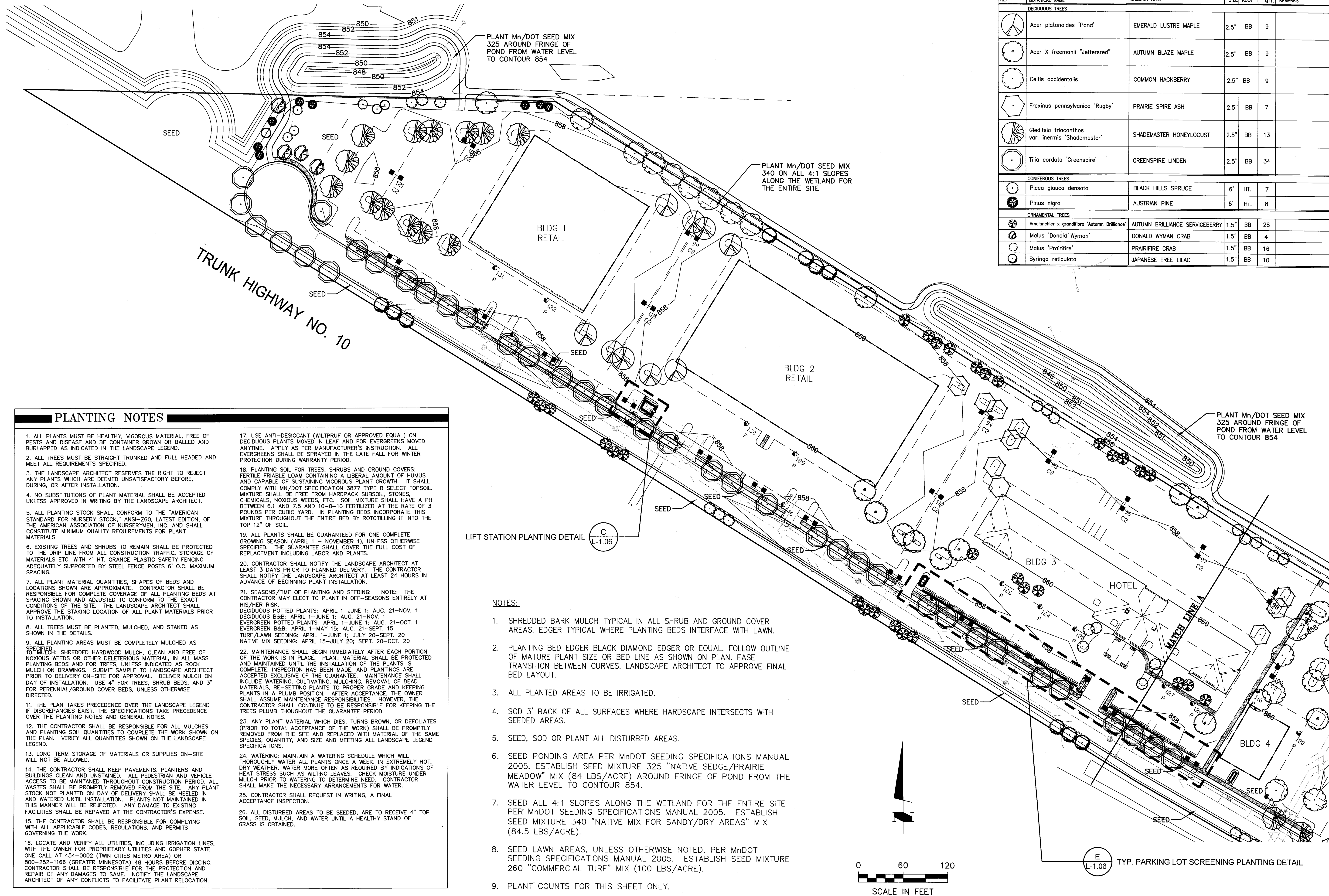
GATEWAY  
COMMERCE  
CENTER  
HANSON BLVD. & HWY. 10  
COON RAPIDS, MN

OVERALL SITE  
PLAN

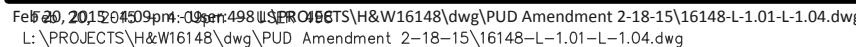
PROJECT NUMBER: 04-1014-01  
ISSUED DATE: 02-16-15  
DRAWN BY: BL/BD  
CHECKED BY: KA

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**Project No. H&W16148**

1. ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE, AND BE CONTAINER GROWN OR Balled AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
2. ALL TREES MUST BE STRAIGHT TRUNKED AND PLANT HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
3. THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ALL PLANTS WHICH ARE OF UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
4. NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
5. ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-260, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
6. EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4' HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6' O.C. MAXIMUM SPACING.
7. ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
8. ALL TREES MUST BE PLANTED, MULCHED, AND STAKED AS SHOWN IN THE DETAILS.
9. ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
  - a. MULCH: SHREDED HARDWOOD MULCH, CLEAN AND FREE OF NOIDS, WEEDS OR OTHER DELETERIOUS MATERIAL IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR TREES, SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
10. THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN. VERIFY ALL QUANTITIES SHOWN ON THE LANDSCAPE LEGEND.
12. LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
13. THE CONTRACTOR SHALL KEEP PAVEMENTS, PLANTERS AND BUILDINGS CLEAN AND UNSTAINED. ALL PEDESTRIAN AND VEHICLE TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION PERIOD. ALL WASTES SHALL BE PROMPTLY REMOVED FROM THE SITE. ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HEeled IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE DAMAGED. ALL DAMAGE TO EXISTING FACILITIES SHALL BE REPAVED AT THE CONTRACTOR'S EXPENSE.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
15. LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND GOPHER STATE ONE CALL AT 454-0002 (TWIN CITIES METRO AREA) OR 800-252-1166 (GREATER MINNESOTA) 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO UTILITIES. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
16. USE ANTI-DESICANT (WILTPROFUR OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WINTERARY PERIOD.
17. PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MN/DOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDLACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5 AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED BY ROTOTILLING IT INTO THE TOP 12" OF SOIL.
18. ALL PLANTS SHALL BE GUARANTEED FOR ONE COMPLETE GROWING SEASON (APRIL 15 - OCTOBER 15) UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
19. CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
20. SEASONS/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK.
  - DECIDUOUS POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-NOV. 1
  - DECIDUOUS B&B: APRIL 1-JUNE 1; AUG. 21-NOV. 1
  - EVERGREEN POTTED PLANTS: APRIL 1-JUNE 1; AUG. 21-OCT. 1
  - EVERGREEN B&B: APRIL 1-MAY 15; AUG. 21-SEPT. 15
  - TURF/LAWN SEEDING: APRIL 15-JULY 20; SEPT. 20-OCT. 20
  - NATIVE LAWN SEEDING: APRIL 15-JULY 20; SEPT. 20-OCT. 20
21. MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE PLANTING IS COMPLETED. ALL WORK SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, WEEDING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
22. ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES PRIOR TO TOTAL ACCEPTANCE OF THE WORK SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
23. WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK, IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATION OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.
24. CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.
25. ALL DISTURBED AREAS TO BE SEEDDED, ARE TO RECEIVE 4" TOP SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.

1. SHREDDED BARK MULCH TYPICAL IN ALL SHRUB AND GROUND COVER AREAS. EDGER TYPICAL WHERE PLANTING BEDS INTERFACE WITH LAWN.
2. PLANTING BED EDGER BLACK DIAMOND EDGER OR EQUAL. FOLLOW OUTLINE OF MATURE PLANT SIZE OR BED LINE AS SHOWN ON PLAN. EASE TRANSITION BETWEEN CURVES. LANDSCAPE ARCHITECT TO APPROVE FINAL BED LAYOUT.
3. ALL PLANTED AREAS TO BE IRRIGATED.
4. SOD 3' BACK OF ALL SURFACES WHERE HARDSCAPE INTERSECTS WITH SEEDED AREAS.
5. SEED, SOD OR PLANT ALL DISTURBED AREAS.
6. SEED PONDING AREA PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 325 "NATIVE SEDGE/PRAIRIE MEADOW" MIX (84 LBS/ACRE) AROUND FRINGE OF POND FROM THE WATER LEVEL TO CONTOUR 854.
7. SEED ALL 4:1 SLOPES ALONG THE WETLAND FOR THE ENTIRE SITE PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 340 "NATIVE MIX FOR SANDY/DRY AREAS" MIX (84.5 LBS/ACRE).
8. SEED LAWN AREAS, UNLESS OTHERWISE NOTED, PER MnDOT SEEDING SPECIFICATIONS MANUAL 2005. ESTABLISH SEED MIXTURE 260 "COMMERCIAL TURF" MIX (100 LBS/ACRE).
9. PLANT COUNTS FOR THIS SHEET ONLY.

### TYP. PARKING LOT SCREENING PLANTING DETAIL





# Planned Unit Development (PUD) Design Guidelines

NW Corner Hanson Boulevard and Highway 10  
Coon Rapids, Minnesota

**Credits & Acknowledgements:**

We would like to thank the City of Coon Rapids planning staff, Planning Commission and City Council as well as the developer's engineers, architects and planners who attended the workshops and meetings and gave their input to shape the planning process. Their input has been invaluable in developing these design guidelines.

**City Council**

Tim Howe, Mayor  
Denise Klint, Ward 1  
Ron Manning, Ward 2  
Paul Johnson, Ward 3  
Joe Sidoti, Ward 4  
Bruce Sanders, Ward 5  
Scott Schulte, At Large

**Planning Commission**

Donna Naeve, Chair  
Tom Greenwood, Commissioner  
Denise Hosch, Commissioner  
Todd Mensink, Commissioner  
Margeret Murphy, Commissioner  
Joy Pirkkl, Commissioner  
Jenny Geisler, Commissioner

**City of Coon Rapids**

Marc Nevinski, Community  
Development Director  
Scott Harlicker, Planner

**Developer & Consultants**

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Properties, LLC.  
Mark Anderson, MFRA, Inc.  
Kathy Anderson, AIA,  
Architectural Consortium, LLC  
Brett Loftesnes, Architectural Consortium,  
LLC.

**Planning & Urban Design Consultant**

Carolyn Krall, AIA, Landform  
Ellen Stewart, ASLA, Landform  
Teresa Forsberg, Assoc. AIA, Landform

**Planned Unit Development (PUD) Site Development Agreement**

The Planned Unit Development (PUD) Design Guidelines are intended to be part of a PUD Site Development Agreement which, together with the approved Preliminary Plat, will define the project scope and act as the guiding plan for the development of this significant corner. The Guidelines are intended to work in conjunction with and in addition to the Coon Rapids City Code standards. All construction is subject to abide by all codes and ordinances as adopted by the State of Minnesota, Anoka County, the City of Coon Rapids and all other pertinent regulations. The design guidelines shall prevail in the event of a conflict.

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## **I. Introduction to P.U.D. Design Guidelines**

**A. Purpose**

The Planned Unit Development (PUD) Design Guidelines are intended to be part of a PUD Site Development Agreement which, together with the approved Preliminary Plat, will define the project scope and act as the guiding plan for the development of this significant corner. The developer intends to develop the site in phases and may sell individual lots or parcels to developers, builders or owner-occupants who would complete the approvals and build-out of their individual lots. The guidelines ensure quality and consistency within the overall development through the process. Guidelines address site design and layout, harmonious style and materials, building massing, materials, architectural features, site landscaping, lighting and signage to establish an overall aesthetic for the Gateway Commerce Center.



The guidelines serve the purpose of identifying elements and design criteria that must be incorporated into the project and communicate the expectations of the City to individual site builders, owners and tenants for each phase within the overall development.

**Framework Principles for Coon Rapids**

This Planned Unit Development should support the overall design principles for the City of Coon Rapids established in the Comprehensive Plan. These principles can be summarized as follows:

- 1) Enhance Coon Rapids' image.
- 2) Improve the public realm.
- 3) Preserve and protect natural resources.
- 4) Encourage complementary, adjacent uses & assets.
- 5) Attract new, desirable uses.
- 6) Expand connectivity for both automobiles and pedestrians.
- 7) Reduce negative impacts of development (*i.e. traffic congestion, stormwater run-off, etc.*).

**B. Submission, Review and Approvals Process**

These Design Guidelines will be finalized by City Staff and approved by the Planning Commission and City Council in conjunction with the Planned Unit Development (PUD) submission. This submission shall include all documentation itemized in the PUD Agreement for the entire development plan. The entire PUD shall be reviewed by the Planning Commission which will make recommendations to the City Council, who will consider approval of the entire PUD. The approved PUD submission documents, including the PUD Agreement and these Design Guidelines, will then form the basis for final site approvals for all or any part of the development. These guidelines are in effect and in conjunction upon approval of the final plat and PUD.

Final approvals for each individual lot within the PUD will go to the Planning Commission for review and final approvals, with appeal available to the City Council. However, proposed modifications which affect the entire PUD, such as road realignment or lot line adjustments, or any other substantial alterations to signage, lighting or landscape plans, will require City Council approval.

**C. Introduction and Overview**

These Design Guidelines are intended to provide a framework for the future development of this significant site in Coon Rapids. The Design Guidelines should provide a baseline for quality



development that meets the community's goals.

**Goal 1:** Preserve and protect natural resources

**Goal 2:** Respect the existing context while encouraging new, contextual responses

**Goal 3:** Establish a baseline for design and quality

**Goal 4:** Provide a unified, overall plan and image to be consistent with phased development

## **Objectives**

The specific objectives for these Design Guidelines are:

- To establish a clear definition of urban design standards that informs property owners, developers, builders, tenants and current and future decision-makers
- To serve as a tool that planners and designers can use and incorporate into their plans to achieve and support the City's design objectives
- To act as a basis for a uniform and consistent review of development and redevelopment proposals by the City Council and Planning Commission

## **Format**

This document is written with Intent Statements, detailed Guidelines and Illustrations. The Intent Statements present the overall goal for development and describe a desired outcome within the site or building design category. The Guidelines provide specific directions on how the intent should be met. The Illustrations are either sketches that further illustrate the Guidelines or photos presenting examples of how the Design Intent has been achieved in other projects.

## **Definitions**

This document is written in common English with all terms as defined in the Merriam-Webster on-line dictionary ([www.m-w.com](http://www.m-w.com)). Land use terms shall be the same as defined in the City Code. The following terms are used frequently in the guidelines.

context: *the interrelated conditions in which something exists or occurs*

discourage: *to hinder by disfavoring*

encourage: *to attempt to persuade or to spur on*

shall: *used in laws, regulations, or directives to express what is mandatory*

should: *used to express a request in a polite manner or to soften direct statement*

## **2. Commercial District**

### **A. General**

The Gateway Commerce Center is expected to include a mix of commercial uses – both destination uses and local, neighborhood-oriented retail and business uses. One side of the Center fronts on Highway 10 and has good visibility to support destination commercial uses. The other side fronts on Hanson Boulevard where more neighborhood-oriented commercial and smaller retail uses are appropriate. Hanson Boulevard offers the only vehicular access into the site. A full-access intersection is planned at Northdale Avenue and Hanson Boulevard which is relatively close to the Highway 10 ramp entrance. A right-in-only access, also on Hanson Boulevard, has been approved by Anoka County. The City hopes to be able to provide an additional road connection to the site from the northwest, enhancing the existing access significantly and allowing for a greater variety of commercial uses.



The Center is also adjacent to significant natural resources. North of the site there is an expansive wetland and in the future the City or County may undertake wetland restoration and improvements. Directly across the street from the Center is Lions Park – a local park with trails that are expected to be extended on the north side of the site to connect to the regional trail system. There are residential neighborhoods adjacent to the park and wetlands, so the Center should provide a pedestrian-friendly environment along this significant resource as well as pedestrian connections throughout the site.



1. New development should consider the available site access. Uses shall not exceed the assumptions included in the Traffic Study for the Business Center in Coon Rapids, December 2004, which formed the basis for the design of the Hanson Boulevard and Northdale intersection improvements. Uses which exceed those assumptions and would result in an overall intersection level of service (LOS) below D, measured at either peak AM or peak PM hour, at the Hanson Boulevard intersections are not acceptable.
2. All new development should be sensitive to existing residential in the design and location of new uses and buildings with respect to shading, views, light and noise pollution; where necessary, appropriate screening or buffering shall be provided.
3. New development shall provide for pedestrian as well as vehicular circulation and shall create linkages to existing and proposed sidewalks and trails.
4. The northwest corner of Hanson Boulevard and Highway 10 is a key point that has been identified as a primary entrance to Coon Rapids and should be designed as a “gateway” to the City. Gateway elements could include both City and development identity features, incorporating signage and unique landscaping, walls or gates, water features and open space which offer views to the adjacent wetlands and ponds.
5. New development should be designed to value, respect and preserve identified natural features and resources. Natural amenities shall be incorporated as features within public spaces through innovative planning and design approaches.

## 1) Land Use

### Intent:

To welcome a mix of high quality office, hospitality, service and retail uses -- both destination uses, and local neighborhood retail and business uses. Land uses should be diverse yet complementary and provide places for residents to meet and socialize.



- a) Development should include a range of commercial uses appropriate to the Coon Rapids area including office, hospitality, retail, service and other commercial uses.
- b) Large scale commercial uses should be concentrated along the Highway 10 entrance ramp, rather than on Hanson Boulevard.
- c) A mix of diverse yet complementary uses appropriate to this area should be located closer to Hanson Boulevard; neighborhood-oriented retail and service commercial uses that support the community and nearby residential neighborhoods such as drug stores, banks, specialty markets and local retailers are encouraged.
- d) Retail or service uses whose primary use includes tattooing, branding, body piercing, or uses subject to licensing under Title 5 of the Coon Rapids City Code, except for the sale of alcoholic beverages or the operation of a therapeutic massage enterprise, shall not be allowed.

- e) Destination, hotel, restaurant, recreational and entertainment uses, particularly mixed with compatible, complementary or other synergistic uses, are encouraged. Automotive repair/service uses or uses with excessively high traffic generation are discouraged.
- f) Buildings 4, 5, and 6 will be “full-service, sit-down” restaurants, where food is both ordered and served by wait staff. Drive-through service will not be provided at these locations.
- g) Uses appropriate to this area that enhance their business by providing drive-through services (i.e. convenience food/gas stations, banks, coffee shops and pharmacies) are permitted provided they are designed to minimize impact on local traffic circulation and limit conflicts with pedestrian access.
- h) Parking lots shall be allowed only as an accessory use, except for shared ramp parking which shall be limited to serving on-site uses. Shared parking is encouraged (see section which follows on parking and services areas).



## 2) Streets and Public Spaces

### Intent:

To provide a well designed public realm that combines access and safety for pedestrians and vehicles while enhancing connectivity between businesses, retail shops, restaurants, parking and public spaces through a logical, safe and well marked, vehicular and pedestrian circulation system.

- a) The central street through the development shall have a 60' minimum right-of-way (ROW) which shall include the following:
  - (i) Drive lane width not more than 36 ft., west of building seven to reduce impervious surface, discourage larger trucks and encourage traffic to slow down, especially through the more active areas of the development;
  - (ii) Sidewalks not less than 5 ft. wide on both sides of the street at the Hanson Boulevard entrance. Sidewalks not less than 5 ft. wide on one side of the street in locations where there are uses only on one side of the street;
  - (iii) Planted boulevard buffer 6 ft. wide between the curb and sidewalk with street trees to provide shade and a sense of scale. This boulevard may be reduced or terminated approaching intersections to allow for pedestrian crossings;
  - (iv) Street lighting that meets the design standards and provides for both street lighting and pedestrian lighting on sidewalks to add character to the street corridor;
  - (v) Where appropriate, areas for seating, street furniture or other pedestrian amenities such as benches, planters, walls, fences, bollards, banner poles, bike racks and other elements that enhance the public realm. These elements shall be managed and maintained to reduce visual and physical clutter while elevating the quality of the pedestrian environment;





- (vi) Where necessary, such as where the street ROW does not allow for a wider sidewalk; these amenities may need to be accommodated within the parcel boundaries.
- b) All street intersections shall be safe for pedestrians and include enhancements to safety such as textured-colored paving or striping to mark crosswalks, narrowed or throated intersections, pedestrian-oriented street lighting, landscaped boulevards buffering the sidewalks from busy streets and pedestrian crossing warning signage at major crosswalks and all trail crossings.
- c) The development shall provide pedestrian connectivity between uses (businesses, parking, open space, etc.) with a designated and well marked pedestrian circulation system that encourages parking and walking rather than driving to each destination within the center, as designated in the pedestrian trail plan.

### 3) Open Space, Trails and Stormwater Management

#### Intent:

To enhance and protect adjacent, natural resources while offering the public enhanced access, connectivity and preserved views along the edge of the existing wetlands and creek.



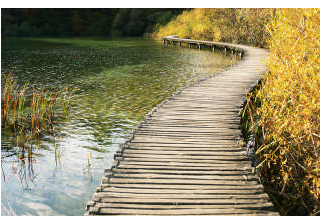
- a) The development has received approvals for stormwater management and the required ponds, located outside the lot lines, are shown on the master plan. Pond edges and landscaping should appear as natural as possible, discourage geese, prohibit wading and reduce algae and other unattractive, surface accumulations.
- b) Site development should encourage the reduction of impervious cover and increase on-site infiltration to reduce potential non-point, source pollution and contaminants from stormwater runoff, where feasible.



- c) Maintain the minimum 10 ft. parking setback on the northern edge of the development along the wetlands as an open-space corridor connecting the site from north to south. The City intends to work with the County to seek approvals for a future pedestrian boardwalk and/or trail as a pedestrian amenity along the north edge of the property. To coordinate with this future amenity, the open space corridor should include:



- (i) At least two paved pedestrian connections between this corridor (future boardwalk) and the sidewalks along the central street, as appropriate;
- (ii) An eight foot wide paved bicycle trail connection extending from this corridor along the north side of the central street to the Hanson Boulevard intersection, where the City intends to connect the trail to the existing bicycle trail at Lions Park;
- (iii) Well defined pedestrian connections pedestrian-oriented destination uses such as retail, cafes or restaurants; these connections may be shared between lots or uses;



- (iv) Pedestrian amenities where the paved bicycle trail connects to this corridor that could include places to rest with benches, bike racks, pedestrian-oriented lights, banner poles, planters, shade trees and similar elements;
- (v) Landscaping that acts as a year-round buffer, and make a harmonious transition from the more groomed landscaped areas to the more natural landscape of the wetland edges.

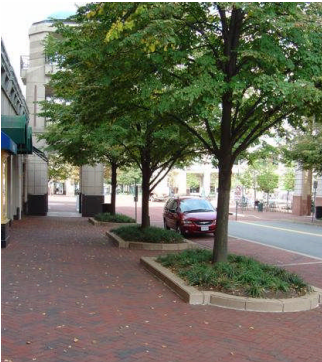
#### 4) Landscaping and Screening

Intent:

To provide landscaping that enhances the public realm, preserves natural areas including areas with wetlands and contiguous, significant native vegetation, maximizes wildlife habitat corridors and water quality and provides a focal point for development. Landscaping plans for the entire PUD and each individual site shall meet the requirements of the City's landscaping standards for commercial developments to the extent possible and the goals of this section.



- (a) Where possible, utilize trees, shrubs and plant materials indigenous to this geographic location and soils, providing habitat value and promoting biodiversity through avoidance of monoculture plantings.



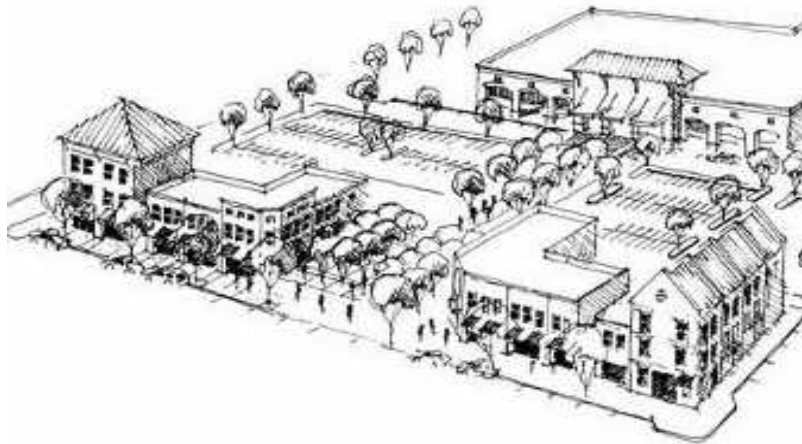
- (b) Maximize green space along the open-space corridor. Open space shall be planted with trees, shrubs, perennials and grasses to enhance the buffers for adjacent areas, reduce maintenance, reduce nitrogen usage and reduce water demands for irrigation while providing a focal point for development.

- (c) Provide street trees to frame the street and provide shade and scale with planting areas that are a minimum of 8 ft. long x 4 ft. wide to allow trees to grow to acceptable sizes. Structural soils (Cornell University Certified or equal) shall be the planting medium for all street trees.



- (d) Use a mixture of vegetation, berms, walls and fencing to screen areas such as parking lots, mechanical systems, transformers and loading docks. Screening materials shall be consistent with the building materials and other elements in the development. Fencing or a mechanical vent screen alone is not sufficient.

- (e) Off-street, surface parking areas shall include landscaped areas equivalent to 30 sq. ft. of landscaping for each parking space in the parking lot or not less than 10 percent of the area of the parking lot. Required landscaping shall take the form of planter strips, landscaped areas and perimeter landscaping. The landscaping shall be dispersed throughout the parking lot and must extend along the entire frontage of adjacent streets except for entry points. Planter strips shall be a minimum of five ft. in width.



- (f) Building facades and exposed foundations shall be articulated with landscaping to provide visual interest and soften appearances.
  - (i) Ornamental plant materials may be used such as flowering trees, shrubs and perennials.
  - (ii) Plantings shall be massed and scaled as appropriate for the building design.

## **B. Site Development**

The overall development plan should be well laid out to ensure that all uses have good access, well located parking, safe, attractive pedestrian connections and adequate visibility. The development should be pedestrian-friendly and walkable for everyone whether arriving by automobile, motorcycle, bicycle, bus or foot.

### **I) Building Orientation**

#### Intent:

To position buildings in a way that responds to climate and site conditions, provides good visibility to improve way-finding and enhances pedestrian accessibility.

- (a) All new development shall be sensitively designed and sited so as to preserve the key vistas from Hanson Boulevard through the site to the wetlands and creek.
- (b) Buildings shall be oriented and designed to consider sun and shade, wind exposure and snow drifting to avoid creating icy or dangerous conditions.
- (c) Building and paving setbacks shall be not less than 10 ft. on the wetland edge. Building setback shall not be less than 30 ft. from the Hanson Boulevard right of way and paving setbacks shall not be less than 10 ft. from the edge of the sidewalk easement.
- (d) Buildings along Hanson Boulevard shall be oriented to face the central parking area with well articulated facades. Service areas shall not face Hanson Boulevard.



Where commercial retail uses occupy the first floor, traditional storefronts (doors, display windows, or both) are encouraged to face towards the sidewalk.

- (e) Building entries shall be both visible and accessible by pedestrians from the main street (Hanson Boulevard or the new central street) and easily distinguished from the building facade; entrance areas shall be well lit, covered or protected from weather and include significant glazing and glass doors where the public will be entering the building.
- (f) Buildings may provide a second entrance from a side or rear parking lot, but the street-front entry must remain open during business hours.
- (g) Buildings shall be oriented to minimize views of service areas (service doors, equipment and loading docks) from public streets; access to these service areas needs to be provided at the alleys, sides or backs of the buildings.

## 2) Site Coverage

### Intent:

To minimize impervious surfaces and stormwater run-off by reducing building footprints, reducing paved parking areas and providing more permeable surfaces.



- (a) Building footprints and site coverage shall be minimized through the use of multiple stories and more efficient layouts where feasible.
- (b) Impervious surface or hardscape should be minimized by reducing the width of drive lanes and parking lots and encouraging central structured parking or shared parking where possible. The developer is responsible for demonstrating parking adequacy based on time of use and demands of individual and surrounding uses.
- (c) Alternative materials such as permeable pavers, porous asphalt, vegetated roof areas, especially on the parking deck, and other innovative techniques to reduce stormwater run-off shall be encouraged.

## 3) Parking and Service Areas

### Intent:

To encourage better site design including improved parking lot efficiency and ease of use while reducing the area of paved surface required and minimizing negative impacts of parking and service areas.



- (a) Parking lots shall be laid out for both efficiency and ease of use with entrances and exits clearly visible and marked.



- (b) Parking spaces should be a minimum of 9 ft. wide and 18 ft. long with a back-up area of not less than 24 ft. Up to 20% of the spaces provided in any parking lot may be designated for compact cars with parking spaces 16 ft. long.



- (c) Where appropriate and feasible, shared parking is encouraged to take advantage of varying parking demands between mixed uses and reduce the overall parking area on the site. The developer is responsible for demonstrating parking adequacy based on time of use and demands of individual and surrounding uses, using national standards for parking demand (such as ITE, APA, ULI, etc). If at any time, the planned uses change, the shared parking calculations would need to be reconsidered based on the proposed new uses.

- (d) Parking lots and driveways along Hanson Boulevard should be minimized. Where a driveway, parking lot or parking deck located along Hanson Boulevard should be well landscaped, screened and buffered with a combination of fencing, walls and plantings along the edge.

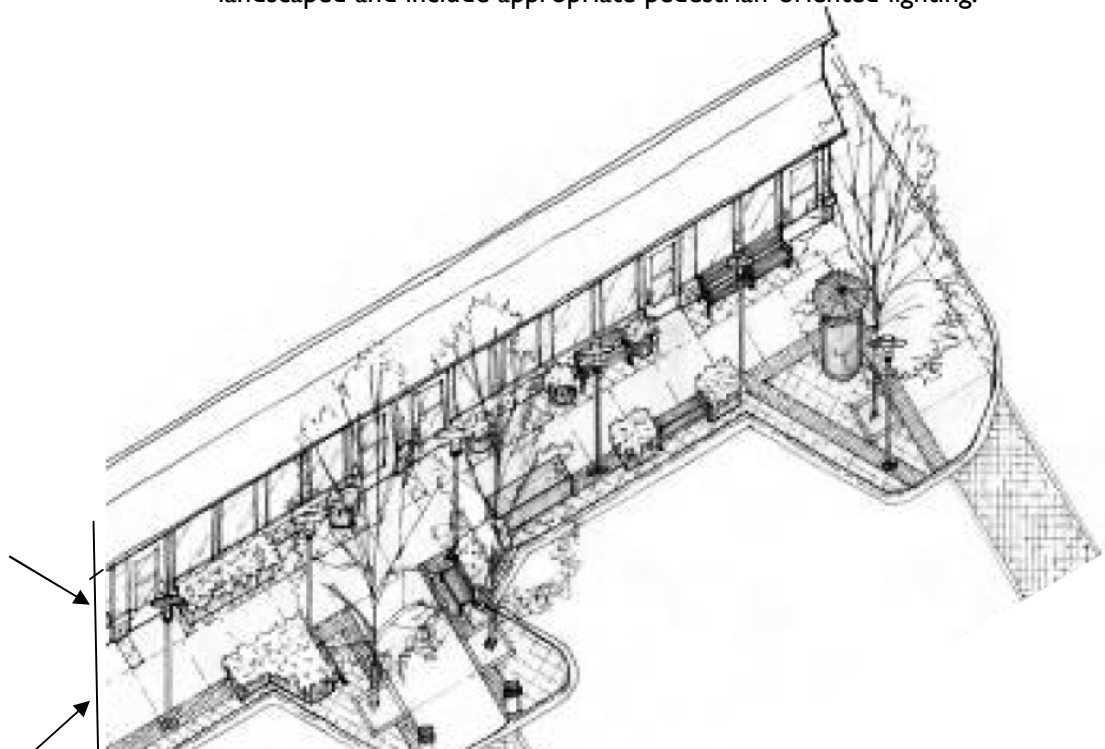
- (e) Where a parking lot fronts on the central street, Hanson Boulevard or the open space corridor, the lot shall be well landscaped and screened. Screening materials should include, singularly or in combination, ornamental fencing, walls, berms and plantings along the street edge.

- (f) Parking bays with an excess of 30 vehicle capacity shall be sub-divided with internal landscaping and pedestrian sidewalks or walkways to create the appearance of smaller parking areas. A bay consists of two rows of parking stalls nose to nose.

- (i) Pedestrian walkways should be well defined (raised, paved and/or marked) and provide pedestrians a safe route to a sidewalk, trail, public space or the entrance of a commercial building.

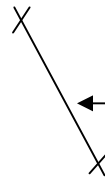
- (ii) Pedestrian walkways provided in larger parking lots should also be landscaped and include appropriate pedestrian-oriented lighting.

Shop-related  
furniture &  
amenities zone



5'-6' Walking Area →

Public Area:  
trash receptacles,  
newspaper stands,  
streetlights, signs  
& street trees  
planted where  
sidewalk widens



← Safe, well defined pedestrian  
walkways across driveways and in  
larger parking areas.



- (g) Parking decks or structures shall not be located along or front on the open space corridor. Parking decks or structures fronting Hanson Boulevard shall be designed to minimize the apparent height of the ramp and the top parking deck shall not be more than 3 ft above the most adjacent Hanson Boulevard sidewalk elevation. Landscaped screening requirements are the same as for other parking lots (see above) along Hanson Boulevard.



- (h) Buildings should be oriented to minimize views of service areas, service doors, meter banks and equipment and loading docks from the street. In addition, these areas should be screened with a combination of solid screening (walls or fences) and landscaping.



- (i) Screening materials should be of the same style, materials and palette as the buildings; metal fencing or a mechanical vent screen alone is not sufficient.

- (ii) Access to these service areas needs to be provided at the alleys, sides or backs of the buildings.



- (i) All mechanical equipment and trash areas shall be fully screened and coordinated with the architecture of the buildings, using the same materials and construction type as the buildings. Free-standing enclosures are discouraged and interior service areas are encouraged.

### C. Architecture

The architecture of the development and all new buildings should be respectful of and relate well to the existing context – natural and man-made – in massing, materials, style and color that expresses complementary design and an appropriate sense of scale.

## **1) Architectural Character**

### Intent:

To define the quality and character of the architecture for all new buildings and structures within the development.



- (a) Buildings should be designed to relate well to the existing natural and man-made environment in massing, materials, style and color. The design of the landscape and hardscape elements should be well coordinated and harmonious with the building design and materials.
- (b) Buildings should be designed in an appropriate, regional style using materials appropriate to the Coon Rapids area; the appearance of native stone is preferred (or a suitably durable material with an appearance of native stone) for landscape walls, monument signs, signage bases, architectural features, decorative hardscape elements and for a building base or foundation material.
- (c) Parking decks and their access ramps or structures shall be designed to harmonize with the buildings they serve in layout, massing, materials, color and overall architectural quality and character in order to minimize their visual impact.
- (d) Corporate chains shall customize their architecture as needed to express their design “brand” in a way that does not dominate the development and harmonizes with the overall development context, colors, materials and style.
- (e) Green building and site design and sustainable design, construction and maintenance practices are encouraged, where feasible.

## **2) Building Massing and Form**

### Intent:

To define the quality and character of the architecture for all new buildings and structures within the development.



- (a) The development shall allow for a variety of building heights and masses as appropriate and feasible for the proposed uses. Multi-story buildings are more appropriate along Highway 10 and close to the Highway 10 and Hanson Boulevard corner than directly on Hanson Boulevard. Building massing should encourage views out to, and over the wetlands amenity.
- (b) Building massing and design elements should include an expression of a building base (ideally with the appearance of stone), a middle and a top with an expressed cornice line at the top of the first floor and the top of the building.
- (c) Buildings should be designed using the same quality materials on sides that are visible from public streets. Rear or non-street facing facades shall include designs and materials that relate to the front or street-facing facades.
  - (i) All facades shall be articulated with a variety of materials, glazing, awnings or other details to add visual interest.



- (ii) Roof lines and cornice details shall be completed in a three-dimensional manner so that the back of walls, roof features or any other unfinished areas are not visible.

- (iii) Buildings may have expressed front façades with unique designs or more significant amounts of glazing, decorative elements or materials.



- (d) Building design shall emphasize the pedestrian experience through the use of architectural features at the street level including awnings, canopies, stoops, porches, decorative light fixtures and material details to add scale and interest.
- (e) Drive-through facilities shall have a roof or canopy cover which is integrated into the architectural design of the building or made of materials which complement the architectural design.

### **3) Exterior Facades, Materials and Color Palette**

#### Intent:

To establish requirements for the exterior materials, finishes and overall palette which support the quality of architecture within the development.



- (a) Buildings and site hardscape shall be constructed and maintained for permanence and longevity – durable, maintainable materials shall be used. Temporary buildings, metal or wood sheds, etc. shall not be permitted.
- (b) Buildings shall be designed with quality materials on all sides and all facades shall be articulated with a variety of materials, glazing or details to add visual interest. Materials shall include a variety of colors and textures that complement each other.
- (c) Buildings shall not have extensive areas of blank walls. Where windows or entrances are not possible, the architecture shall include varying walls, planes, materials, textures and colors to produce a better articulated and livelier building façade.
- (d) Signage and lighting that are integrated into the design of the building are encouraged (*see next section*).
- (e) Signage and lighting of corporate-branded buildings shall be designed to be integrated into the architecture of the building and the surrounding development, and not dominate the development.



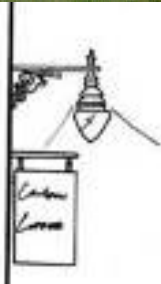
## D. Lighting and Signage

Lighting and signage for the Center should be appropriate for the uses planned and oriented as appropriate to the highway, the boulevard and pedestrian routes into and through the site. Lighting and signage shall be designed, sited and lit in ways that do not distract drivers, create safety concerns or cause undue impact on the adjacent park and residential neighborhoods.

### 1) Lighting

Intent:

There should be a coordinated approach to lighting throughout the site that is appropriate for the uses planned and designed to have minimal impact on adjacent uses. Pedestrian-oriented lighting will be required on all streets, trails and sidewalks and public gathering places within the Center.



- (a) Lighting shall provide a well coordinated overall approach and incorporate both fixtures and lamps of similar types and colors throughout the development. A lighting plan and fixture specifications shall be included in the documents submitted to the City for approval.
- (b) Architectural lighting, particularly decorative, accent lighting, is encouraged to highlight building character, massing and design features, particularly where the building meets the pedestrian realm.
- (c) Building entries with commercial or public uses shall be illuminated. Appropriate design approaches include washing the entry surfaces and doorways with light, allowing building interior light to glow through glazing, or using well designed and directed decorative lighting attached directly to the building.
- (d) Buildings that front areas of high pedestrian activity, particularly along the wetland's public edge, are encouraged to use identity lighting to enliven the space. Identity lighting includes accent lighting for the illumination of signs, awnings or graphics as well as interior lighting in storefront windows. Appropriate design approaches include decorative lighting fixtures attached directly to the building, focused spot lighting on signs, awnings, or window displays and interior decorative or identity lighted signs such as neon that are positioned to shine through glazing.
- (e) Trails shall be provided with a minimum of lighting required for the safety of pedestrians, but it is generally assumed that park areas will not be used extensively after dark. Specific trail lighting is not required except where trails cross streets, access driveways, cross other trails or where pedestrian-oriented lighting should be provided for safe crossings.
- (f) Street lights, standards and bases should be functional and, coordinated with the, color palette, style and materials of the development, and be consistent throughout. Double-headed fixtures are recommended where both the street and pedestrian sidewalks are to be illuminated; otherwise, a single-headed fixture is acceptable. Fixtures and lamps shall be designed with a sharp cut-off angle and

shielded to direct light toward the ground to avoid shining unnecessary light upward to the sky.

- (g) Parking lots shall be lit with the same or similar style fixture as used for the street lights. Fixtures and lamps shall be designed for a wide-coverage area with a sharp cut-off to reduce both the number of fixtures required and glare of light spillage from parking areas.
- (h) All lighting fixtures, including spotlights, electrical reflectors and other means of illuminating signs, structures, landscaping, parking, loading and similar areas, shall be focused, directed and shielded to prevent horizontal glare, direct illumination of adjoining property or streets and upward light leakage.
- (i) Canopies shall not be lit, and lighting under covered drives or canopies shall not exceed the lighting levels appropriate for the functions and uses below; all fixtures shall be fully recessed, directed to reduce glare and screened to reduce visibility from the street. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center's standard fixtures.
- (j) Lighting within parking decks shall not exceed the lighting levels appropriate for parking areas; all fixtures shall be located to reduce visibility from the street and screened to prevent glare. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center's standard fixtures.
- (k) Security and service lighting shall be provided at all service areas and entries. Wherever possible, such lighting shall be controlled by a motion detector so that lighting is provided where and when it is needed for individual safety. Fixtures shall be building-mounted and designed to direct the light where needed and will not produce glare or light leakage. Fixtures shall be coordinated with the overall fixture types (style, materials, color and finishes), and lamps shall be of the same type and color as the lamps used in the Center's standard fixtures.
- (l) No mercury vapor utility lights or other light fixtures with high-intensity discharge lamps or bulbs that are not designed to limit, control light direction or shield the light source from view of neighboring residential properties shall be permitted.



## 2) Signage

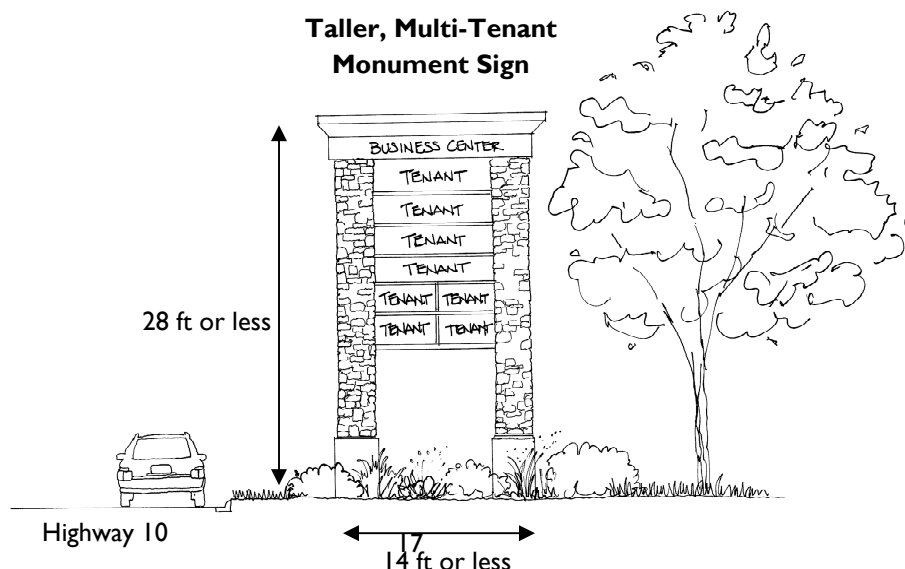
Intent:

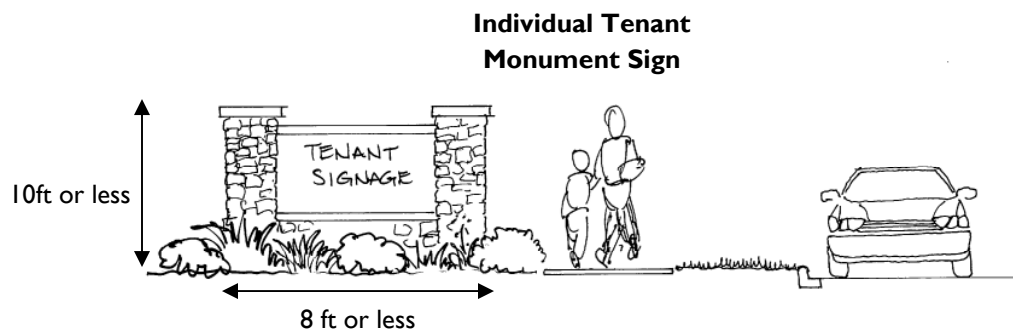
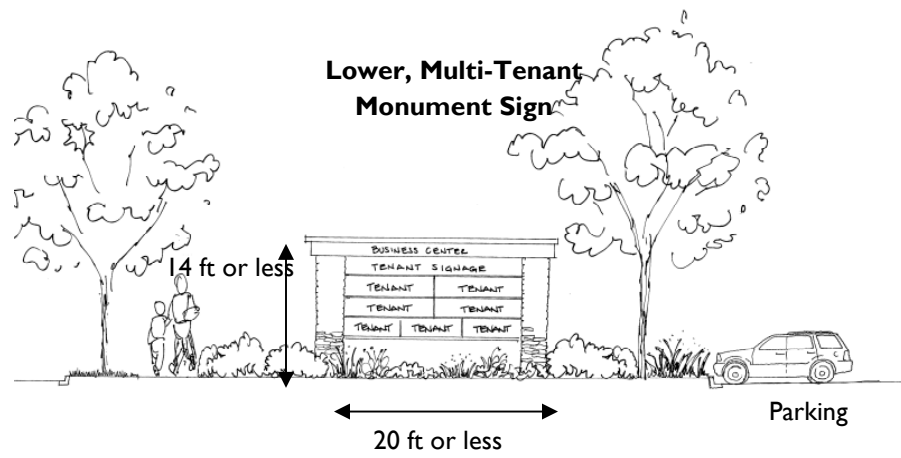
Signage is critical to both the character and the success of the Center, especially at night, so all signage (type, size, placement, materials, etc.) is subject to careful review by the City and must comply with the design guidelines as well as all zoning and signage ordinance requirements.

I.



- (a) All major site signage, as well as provisions for individual building signage and tenant signage, shall be included in the documents submitted to the City for overall site approval and in final approval submissions.
- (b) The Hanson Boulevard and Highway 10 corner is a prominent corner in the City and the Center shall locate an appropriate, welcoming identity element with signage, landscaping and lighting on the corner. This element may be integrated into the design of the corner building or parking ramp. Similar elements with signage, architectural elements, landscaping and lighting may be located at each street access or entrance to the site and shall be consistent with the primary corner feature.
- (c) The Center may locate appropriate monument identity signs on each street frontage; these monuments shall be of the same materials and color palette as the architecture of the Center, with the appearance of stacked native stone preferred. These signs may identify both the Center and individual tenants, as follows:
  - (i) Taller, multi-tenant monument sign located along the Highway 10 edge of the development (only one is allowed for the overall development, plus one such sign for a hotel); this sign shall be not more than 28 ft. in height and not more than 14 ft. in width and the actual back-lit sign area shall not exceed 180 sq. ft. per side in area. The sign shall be setback 50 ft. from the Highway 10 ROW;
  - (ii) Lower multi-tenant monument sign located on Hanson Boulevard (not more than two are allowed); this sign shall be less than 14 ft. in height and less than 20 ft. in width, and the actual back-lit sign area shall not exceed 140 sq. ft. in area per side;
  - (iii) Individual tenant signs (not more than one per lot is allowed); this sign shall be less than 10 ft. in height and less than 8 ft. in width, and the actual back-lit sign area shall not exceed 50 sq. ft. in area per side. These signs shall be located on the same lot as the tenant. However, the convenience store is allowed to locate its tenant sign along Hanson Boulevard.









- (d) Monument signs shall be spaced not less than 100 ft. apart; and shall be placed so that they do not impact safety by blocking motorist views of pedestrian crossings or oncoming traffic at intersections and driveways.
- (e) All monument signs shall be set back 10 ft. from any street ROW.
- (f) Exterior signage shall be fixed and stationary with no electronic, flashing, changing or moving elements. *(coordinate with City policy re: moving signs)*
- (g) Building signage and graphics shall be integral to the building architecture or used as a decorative, design element. More traditional awnings, hanging signs or architecturally integrated signs that are lit by traditional lighting fixtures (spots, sconces, etc.) are encouraged. Large areas of back-lit signage are discouraged and back-lit awning or canopy signs are not acceptable.
- (h) Interior signage, such as decorative neon, moving or other decorative sign types are desirable in limited size when displayed within storefront windows in the interiors of buildings.
- (i) Corporate chains are encouraged to customize their architecture to express their design “brand” within a design, context and material appropriate to the character of the development and should not rely solely on corporate signage. Corporate signage shall be high quality and well integrated into the design of the buildings in a way that does not detract from the overall design of the development.
- (j) The development shall provide both auto and pedestrian way-finding signs and other environmental graphics to supply directions to the pedestrian. This shall be done in a comprehensive and unified manner for the entire development. The proposed graphics should be included in the submission documents for final approvals.

[End of Design Guidelines]



**City Council Regular**

**15.**

**Meeting Date:** 04/07/2015

**Subject:** Consider Approval of Master Contracting Agreement for Citywide Emergency Repairs

**From:** Tim Himmer, Public Works Director

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**INTRODUCTION**

From time to time, the City retains contractors to assist with various citywide repair projects; including such items as watermain breaks, sanitary sewer repairs, drainage enhancements, etc. Typically the City contracts with up to three firms with expertise in specific areas that cannot be provided by City forces. Due to the emergency nature of the repairs, and as opposed to relying on a time and material proposal for each specific project, staff is requesting that the City Council consider a master agreement that outlines the process that will be undertaken for such repairs and provides annual pricing rates.

**DISCUSSION**

For several years the City has retained contractors to provide specialized emergency repairs. A Master Contracting Agreement has been prepared to formalize the City's relationship with the various contractors, provide consistency in the approach to resolve needed repairs, speed up the process to ensure items are addressed in a timely manner, and allow staff to better manage the services provided by the various contractors the City retains.

**RECOMMENDATION**

Staff recommends that the Council approve the Master Contracting Agreement, and authorize City officials to execute the attached agreement with Dave Perkins Contracting, Inc. for professional contracting services beginning 2015 and beyond.

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**Attachments**

Master Agreement

Letter of Engagement

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## CITY OF COON RAPIDS

### MASTER CONTRACTING AGREEMENT

This Agreement ("Agreement") is entered into this 12 day of March, 2015, by and between the City of Coon Rapids, 11155 Robinson Drive NW, Coon Rapids, MN 55433 (the "City") and Dave Penning Contracting, Inc., (the "Contractor").

**WHEREAS**, Contractor has experience in construction activities and is a licensed qualified professional construction/contracting firm; and

**WHEREAS**, the City desires to engage the Contractor from time to time to assist in providing construction/contracting services for projects designated by the City and as described through a separate letter of engagement attached hereto as Exhibit A (the "Letter of Engagement"); and

**WHEREAS**, Contractor desires to accept such engagement upon the terms and conditions hereinafter set forth.

**NOW THEREFORE**, in consideration of the mutual agreements herein contained and intending to be legally bound hereby, the City and Contractor hereby agree as follows:

#### **ARTICLE 1. TERM OF CONTRACT**

Section 1.01. **TERM**. This Agreement will become effective on March 12, 2015 and will continue in effect, unless terminated in accordance with the provisions of Article 7 of this Agreement.

#### **ARTICLE 2. INDEPENDENT CONTRACTOR STATUS**

Section 2.01. **INDEPENDENT CONTRACTOR STATUS**. The City and Contractor expressly agree that Contractor is an independent contractor and not an employee, agent, joint venturer, or partner of the City. Nothing in this Agreement shall be interpreted or construed as creating or establishing the relationship of employer and employee between the City and Contractor or any employee or agent of Contractor. Both parties acknowledge that Contractor is not an employee of the City for state or federal tax purposes.

#### **ARTICLE 3. SERVICES TO BE PERFORMED BY CONTRACTOR**

Section 3.01. **CONTRACTOR'S SERVICES**. The City hereby retains Contractor, on a non-exclusive basis, for the purpose of advising and contracting with the City on matters related to, but not necessarily limited to some or all of the following services:

- Emergency watermain breaks
- Emergency sanitary sewer repairs

- Curb stop, gate valve, and hydrant repairs
- Drainage enhancements and flood protection projects
- Conduit installation
- Directional boring
- Equipment rentals (including laborers)
- Drilling services
- Miscellaneous specialty/technical services identified by the City

(the “Services”).

The precise scope of services, schedule, and compensation on any given project shall be as detailed in the Letter of Engagement issued by the City. The Letter of Engagement may incorporate some or all of the Services, as well as other additional services that may not be identified within this Agreement.

Professional services provided under this Agreement will be conducted in a manner consistent with that level of care and skill ordinarily exercised by members of the Contractor’s profession currently practicing under similar conditions. No warranty, express or implied, is made.

Section 3.02. **METHOD OF PERFORMING SERVICE(S)**. Contractor shall recommend to the City, for its approval, the method, details, and means of performing the selected Service(s). Said scope of services shall be included in the specific Letter of Engagement for a particular project. In general, it is understood that the City will supply all materials necessary to perform the work and Contractor will provide the equipment and labor.

Section 3.03. **PLACE OF WORK**. Contractor shall base the performance of the selected Services at its own business location or such other location as Contractor may determine. The City is not required to provide office, technical or clerical support services to Contractor; but the City at its discretion may chose to do so for specific tasks and/or projects. Work under this Agreement will occur at various locations within the city of Coon Rapids; generally located within the public right-of-way and/or easement(s) secured by the City.

Section 3.04. **TIME OF PERFORMANCE**. Contractor shall submit for the City’s approval a schedule for the performance of the selected Service(s) which schedule may be adjusted by mutual consent as the Service(s) proceed. This schedule shall include allowances for periods of time required for the City’s review. Time limits established by this schedule, and approved by the City, shall not be exceeded by Contractor or the City, except for reasonable delays that are outside the control of either entity. Items that may delay the agreed upon schedule must be communicated to the other party as soon as they become known. Said schedule shall be included in the specific Letter of Engagement for a particular project.

## **ARTICLE 4. COMPENSATION**

Section 4.01. **AMOUNT OF COMPENSATION**. The City shall pay the Contractor for the Service(s) furnished, and the Contractor shall accept as full payment, the sum described in the Letter of

Engagement for a particular project. This sum shall include all Service(s) rendered by the Contractor under this Agreement (including all travel, living and overhead expenses incurred by the Contractor in connection with performing the Service(s) herein), except for additional services authorized in writing by the City. A schedule of hourly fees shall be provided to the City by the Contractor on an annual basis.

Compensation for Service(s) will be provided on an "Hourly – Time and Equipment" basis, unless specifically stipulated otherwise. Contractor's level of Service(s) and actual charges may depend in part on such unknown factors as soil conditions, proper notification of affected authorities, Gopher State locates, weather conditions, and unforeseen site conditions that may be revealed during construction. The Contractor shall promptly notify the City if Contractor feels that project conditions have changed or anticipates that the sum may be exceeded, in order to determine whether or not the City is prepared to increase the compensation. Any agreed upon changes shall be in writing between the City and Contractor.

Compensation for a particular project will be determined by applying agreed upon annual rates and charges, and the level of care necessary to complete each particular project.

Section 4.02. **PAYMENT OF COMPENSATION.** Contemporaneously with the submission of any invoice to the City, the Contractor shall provide an itemized statement detailing the number of hours spent by Contractor's staff and/or equipment, or their agent, as well as the hourly rate charged by the individual and/or equipment performing the work. The Contractor's billings shall also include the project location, City project name and number, description of the work performed, contract amount and amount invoiced to date, and contract percent complete and percent remaining. Invoices shall include subtotals for each phase of the project, if required. Each invoice submitted by the Contractor shall include only one project. The Contractor will bill the City monthly. The City will pay the Contractor within 30 days after receipt of the invoice. Prior to the processing of any and all payments, the Contractor shall comply with the City Finance Department's regulations on the completion and filing of W-9 forms and other Internal Revenue Service and Minnesota Department of Revenue forms.

Section 4.03. **EXPENSES.** Contractor shall be responsible for all costs and expenses incidental to the performance of the Service(s), including but not limited to, all taxes required of or imposed upon Contractor and all other of Contractor's costs of doing business. The City agrees to reimburse Contractor only for those reimbursable expenses set forth in the Letter of Engagement for each particular project. Any expenses related to special consulting or technical services (e.g., outside consultants, subcontractors, or technical services) must be pre-approved by the City. When retained directly by the Contractor, with prior approval by the City, Contractor shall bill the City no more than 105% of actual costs of such special consulting, subcontractor, or technical services.

## **ARTICLE 5. CONSULTANT'S OBLIGATIONS**

Section 5.01. **NONDISCLOSURE OF CONFIDENTIAL INFORMATION.** Contractor shall not disclose to any unauthorized person any confidential information it may obtain regarding the City or its methods of

doing business. All confidential information, whether prepared by Contractor or otherwise coming into its possession, shall remain the exclusive property of the City and shall not be used by Contractor except in the course of the performance of Contractor's Service(s) under this Agreement. Confidential Information shall mean any data and information not previously known to and generated by the Contractor or furnished to the Contractor and marked "CONFIDENTIAL" by the City. Contractor shall have no obligation to maintain confidentiality of information for which it has a legal duty to disclose under statute, state or federal rule, or court order, and assumes no liability for release of such information, but will endeavor to advise City of such legal obligation prior to release.

Section 5.02. **STATE AND FEDERAL TAXES.** As Contractor is not the City's employee, Contractor is responsible for paying all required state and federal taxes. The City will not withhold FICA (Social Security) from Contractor's payments; will not make state or federal unemployment insurance contributions on Contractor's behalf; will not withhold state or federal income tax from payment to Contractor; will not make disability insurance contributions on behalf of Contractor; or will not obtain workers' compensation insurance on behalf of Contractor.

## **ARTICLE 6. THE CITY'S OBLIGATIONS**

Section 6.01. **THE CITY'S COOPERATION.** The City agrees to comply with Contractor's reasonable requests necessary for the performance of the Contractor's Service(s) pursuant to this Agreement.

## **ARTICLE 7. TERMINATION OF AGREEMENT**

Section 7.01. **TERMINATION ON OCCURRENCE OF STATED EVENTS.** The City may terminate this Agreement automatically on the occurrence of any of the following events: (1) failure of Contractor, after notice and a reasonable opportunity to cure, to perform work in a timely fashion; (2) Contractor's bankruptcy or insolvency; or (3) the sale or merger of Contractor's business and/or change in majority ownership. Additionally, this agreement may be terminated by either party upon thirty days written notice without cause. In the event of termination, City shall pay Contractor for all undisputed services rendered prior to termination, and copies of plans, reports, specifications, electronic drawing/data files, field data, notes, and other documents, written, printed or recorded on any medium, finished or unfinished, prepared by the Contractor pursuant to this Agreement and pertaining to any work or projects, subject to provisions of Section 8.10, shall be made available to the City. All provisions of this agreement allocating responsibility or liability between the City and Contractor shall survive the completion of the Service(s) and/or the termination of this Agreement.

Section 7.02. **TERMINATION FOR FAILURE TO MAKE AGREED-UPON PAYMENTS.** Should the City fail to pay Contractor all or any part of the compensation set forth in Article 4 of this Agreement on the date due, the Contractor may stop work or terminate this Agreement if the failure is not remedied by the City within thirty (30) days from the date payment is due.

## ARTICLE 8. GENERAL PROVISIONS

Section 8.01. **NOTICES.** Any notices given hereunder by either party to the other shall be in writing and may be effected by personal delivery with signed receipt or by registered or certified mail with postage prepaid and return receipt requested. Mailed notices shall be addressed to the corporate office of the parties appearing in the introductory paragraph of this Agreement. Notices delivered personally or by mail will be deemed communicated as of the date of actual receipt.

Section 8.02. **ASSIGNMENT.** Except for the Contractor's use of necessary outside consultants or subcontractors, the Contractor and the City shall not assign or delegate their respective obligations under this Agreement without the written consent of the other party, which consent shall not be unreasonably withheld.

Section 8.03. **ENTIRE AGREEMENT OF THE PARTIES.** This Agreement supersedes any and all agreements, either oral or written, between the parties hereto with respect to the rendering of services by Contractor for the City and contains all the covenants and agreements between the parties with respect to the rendering of such services in any manner whatsoever. Each party to this Agreement acknowledges that no representations, inducements, promises, or agreements have been made, orally or otherwise, by any party, or by anyone acting on behalf of any party, which is not embodied herein, and that no other agreement, statement or promise not contained in this Agreement shall be valid or binding. Any modification of this Agreement will be effective only if it is in writing signed by the party to be charged.

Section 8.04. **INDEMNIFICATION.** Contractor agrees to defend, indemnify and hold the City, its officers, and employees harmless from any liability, claims, damages, costs, judgments, or expenses, including reasonable attorney's fees, to the extent attributable to a negligent or otherwise wrongful act or omission (including without limitation professional errors or omissions) of the Contractor, its agents, employees, or subcontractors in the performance of the services provided by this Agreement and against all losses by reason of the failure of said Contractor fully to perform, in any respect, all obligations under this Agreement. Contractor further agrees to indemnify the City for defense costs incurred in defending any claims, unless the City is determined to be at fault.

Section 8.05. **INSURANCE.** In order to protect itself as well as the City under the indemnity provision set forth above, the Contractor shall at all times during the term of the Agreement keep in force the following minimal insurance protection in the limits specified:

- A. A single limit or combined limit or excess umbrella general liability insurance policy in an amount not less than \$500,000 for property damage arising from one occurrence, \$1,500,000 for total bodily or personal injuries or death and /or damages arising from one occurrence. Such policy shall also include contractual liability coverage by specific endorsement or certificate acknowledging this Agreement between the Contractor and the City.
- B. A single limit or combined limit or excess umbrella automobile liability insurance policy, if applicable, covering owned, non-owned and hired vehicles used regularly in the provision of services under this Agreement, in an amount of not less than \$500,000 per accident for property

damage, \$1,500,000 for bodily injuries and / or damages to any one person, and \$1,500,000 for total bodily injuries and / or damages arising from any one accident.

- C. A professional liability insurance policy covering personnel of the Contractor while performing Service(s) under this Agreement in the following amounts: Errors and omissions \$500,000 per claim and \$1,500,000 in total.
- D. Workers Compensation Insurance and employer's liability as required by law including all states endorsement in an amount of \$100,000 for each occurrence.
- E. Prior to the effective date of this Agreement, the Contractor will furnish the City with certificates of insurance as proof of insurance for general Liability and Auto Liability. Such insurance certificates shall be updated annually and provided to the City with the Contractor's annual rates and charges adjustments.
- F. Any policy obtained and maintained under this Section 8.05 shall provide that it shall not be cancelled, materially changed, or not renewed without prior notice thereof to the City. The Contractor will endeavor to provide a thirty (30) day advance notice of any such policy revisions.

Section 8.06. **SEVERABILITY**. Whenever possible, each provision of this Agreement will be interpreted in such a manner as to effect and valid under applicable law. If any provision of this Agreement is held to be invalid, illegal, or unenforceable in any respect under any applicable law or rule, in any jurisdiction, such invalidity, illegality, or unenforceability will not affect any other provision or the effectiveness or validity of any provision in any other jurisdiction, and the remaining provisions of this Agreement will continue in full force without being impaired or invalidated in any way.

Section 8.07. **GOVERNING LAW**. All issues concerning this Agreement will be governed by and construed in accordance with the laws of the State of Minnesota, without giving effect to any choice of law or conflict of law provision or rule (whether of the State of Minnesota or any other jurisdiction) that would cause the application of the law of any jurisdiction other than the State of Minnesota. Any disputes between City and Contractor shall be negotiated in good faith for 30 days, and unresolved disputes shall be submitted to mediation prior to either party pursuing their rights in a court of competent jurisdiction.

Section 8.08. **AFFIRMATIVE ACTION**. The Contractor shall not discriminate under the contract against any person in accordance with federal, state and local regulations.

The Contractor shall not discriminate in employment practices on the basis of race, color, creed, religion, national origin, sex, age, marital status, public assistance status, veteran status, handicap or disability; that it has agreed to take affirmative action to recruit minorities, women and handicapped persons into its employment.

The Contractor shall furnish documentation that shows they have adopted a written affirmative action policy. If during the term of the Agreement, it is discovered that the Contractor is not in compliance with the applicable regulations as aforesaid, or if the Contractor engages in any discriminatory practices,



then the City, through the office, may cancel said Agreement as provided by the cancellation clause of the Agreement.

Section 8.09. **ETHICS**. The Contractor certifies that it does not presently have an interest in real estate, development proposals or have a client with development proposals or real estate interests which are in the City or which will directly benefit or be affected by projects they are assigned to Contractor. Furthermore, the Contractor agrees that it will not acquire interest in any real estate of development proposals, or accept a contract with any client owning real estate or having a development proposal in the City or which will be directly affected or benefited by a project without first notifying and discussing said interest or contract with the City. The Contractor may not perform work on behalf of the City on any properties the Contractor owns or has an interest in.

The Contractor shall not accept any private client or project which, by nature, places it in ethical conflict during its representation of the City. To remove any potential or actual conflict of interest, the Contractor representing any private party client submitting a project or activity to the City shall not represent or review the project or activity on behalf of the City.

The Contractor shall maintain records that reflect all revenues, costs incurred and services provided in the performance of this Agreement. The Contractor agrees that the City, the State Auditor, or legislative authority, or any of their duly authorized representatives upon reasonable notice during normal business hours, and as often as they may deem reasonably necessary, shall have access to and the right to examine, audit, excerpt, and transcribe any books, documents, papers, records, etc., and accounting procedures and practices of the Contractor which are relevant to this Agreement.

The Contractor represents that it has, or will secure at its own expense, all personnel required in performing services under this Agreement. Any and all personnel of the Contractor or other persons, while engaged in the performance of any work or services required by the Contractor under this Agreement, shall have no contractual relationship with the City.

Section 8.10. **RIGHT IN WORK PRODUCT**. The work product of Contractor, including data, information, drawings, results, ideas, developments, plans, specifications, reports or inventions, regardless of format or media, which Contractor conceives or reduces to practice during the course of its performance under this Agreement are Instruments of Service ("Instruments of Service"), and Contractor shall retain an ownership and property interest therein (including the right of reuse at the discretion of the Contractor). Upon payment of all amounts owed Contractor grants to the City partial ownership and an irrevocable license to use such Instruments of Service as deemed necessary by the City pursuant to this agreement. Contractor will furnish the City with electronic data versions of all drawings, data files, reports and/or other digital or written documents ("Digital Data") in a form compatible with the City's software requirements if requested by the City. Such information will also be provided in hard copy form if requested by the City. In the event of any conflict between hard copy documents and the Digital Data, the hard copy documents shall govern. The Digital Data shall be prepared in a format required by the City for its use. The City understands that the Digital Data is perishable and the City is responsible for

maintaining it, and agrees that Contractor is not responsible for use of Digital Data distributed by the City to third parties unless such distribution was a specific part of the Contractor's Service(s).

All data collected, created, received, maintained, or disseminated, or used for any purposes in the course of the Contractor's performance of the Agreement is governed by the Minnesota Government Data Practices Act, Minnesota Statutes 1984, Section 13.01, et seq. or any other applicable state statutes and state rules adopted to implement the Act, as well as state statutes and federal regulations on data privacy. The Contractor agrees to abide by these statutes, rules and regulations and as they may be amended.

Contractor makes no representation that Instruments of Service provided for any specific project are suitable for reuse, modification or benefit of City or others on extensions of the Project, modifications or any other project. Any reuse or modification of Contractor's Instruments of Service without written verification or adaptation by Contractor, as appropriate for the specific purpose intended, will be at the City's sole risk and without liability or legal exposure to Contractor. The City shall indemnify and hold harmless the Contractor from all claims, damages, losses and expenses, including attorneys' fees, arising out of any reuse or modification of the Instruments of Service without the participation of the Contractor.

Section 8.11. **HAZARDOUS SUBSTANCE**. The Contractor's scope of services does not include any Service(s) related to hazardous or toxic materials, including asbestos and PCBs. If it becomes known that such materials may be present at or near a project that may affect the Contractor's Service(s), the Contractor must immediately inform the City in writing and may suspend performance of its Service(s), without liability, and will assist the City to retain appropriate specialist consultants to adequately identify and abate such materials so that Contractor's Service(s) may resume.

Section 8.12. **CONSTRUCTION OBSERVATION**. The City will provide a competent person on all job sites while the Contractor performs the requested Service(s). Said competent person will be present on the job site to document the work being completed, monitor the progress and quality of the work, and determine if the work is generally proceeding in accordance with the City's direction. The competent person does not guarantee the performance of, and shall have no responsibility for, the health, safety, means, methods, techniques, acts or omissions of the Contractor or their subcontractor(s), supplier(s) or any other entity furnishing materials or performing any work on the project. The Contractor shall provide a competent site supervisor to provide direction and oversight of their personnel.

Section 8.13 **OPINIONS OF CONSTRUCTION COST**. Where provided by the Contractor as part of Service(s) or otherwise, opinions or estimates of construction cost will generally be based upon public construction cost information and estimated hours to complete the work. Since the Contractor has no control over the cost materials, weather conditions, and other factors affecting the cost of construction, all cost estimates are opinions for general information of the City and the Contractor does not warrant or guarantee the accuracy of construction cost opinions or estimates. The City acknowledges that costs for project financing should be based upon contracted construction costs with appropriate contingencies.

Section 8.14. **ANNUAL REVIEWS.** The City shall conduct an annual review of the Contractor's activities and work product for that year. Such review may be conducted by the City's Public Works Director. A poor evaluation, which cannot be corrected or is determined by the City to be gross negligence, may be grounds for termination of this Agreement.

**IN WITNESS WHEREOF**, the parties hereto have executed this Agreement as of the date first above written.

CITY OF COON RAPIDS:


By: \_\_\_\_\_  
Jerry Koch  
Its: Mayor

By: \_\_\_\_\_  
Steve Gatlin  
Its: City Manager

Approved As To Form

By: \_\_\_\_\_  
David Brodie  
Its: City Attorney

CONTRACTOR:

By:  \_\_\_\_\_  
Its: president  
Dave Perkins

**CITY OF COON RAPIDS**

**LETTER OF ENGAGEMENT**

**Project Name:** \_\_\_\_\_

**Project No.:** \_\_\_\_\_

This Letter of Engagement is entered into this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_ by  
and between the CITY OF COON RAPIDS, 11555 Robinson Drive, Coon Rapids, Minnesota  
55433 (the "City") and



**Dave Perkins Contracting, Inc.**  
**19745 Nowthen Blvd NW**  
**Nowthen, MN 55303-9655**

\_\_\_\_\_  
(Name, Address)

\_\_\_\_\_  
(the "Contractor").

**RECITALS**

WHEREAS, the City and the Contractor entered into an Master Contracting Agreement  
(the "Agreement") on the 12 day of MARCH, 2015; and

WHEREAS, this Agreement provides that the City will engage the Contractor from time  
to time to assist in providing construction services for projects and studies designated by the City  
and as described through separate Letters of Engagement; and

WHEREAS, the City wishes to retain Contractor to perform Service(s) to assist with the  
following described project:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

(the "Project"); and

WHEREAS, this Letter of Engagement outlines the Service(s) to be performed by the  
Contractor; the approved cost of the Project; and the Project schedule.

NOW, THEREFORE, in consideration of the mutual agreements herein contained and  
intending to be legally bound hereby, the City and the Contractor hereby agree as follows:

**ARTICLE ONE**

**SERVICES TO BE PERFORMED BY THE CONTRACTOR**

The City hereby retains Contractor for the purposes of constructing the services described  
on Exhibit A attached hereto and made a part hereof.

## ARTICLE TWO SCHEDULE OF PERFORMANCE

The Contractor shall perform the services for the Project on the basis of the schedule attached hereto as Exhibit B and made a part hereof by reference. The schedule may be adjusted by mutual consent as the Service(s) proceed. The term limits established by the schedule and approved by the City shall not be exceeded by the Contractor or the City, except for a reasonable cause agreed to by the City.

## ARTICLE THREE CONTRACTOR'S COMPENSATION

The City shall pay the Contractor for services furnished and the Contractor shall accept as full payment the sums described on Exhibit C attached hereto and made a part hereof.

## ARTICLE FOUR CAPITALIZED TERMS

Capitalized terms not otherwise defined herein have the meaning given them in this Agreement.

Subject to the terms and conditions of this Engagement Letter, all of the terms and conditions of the Master Contracting Agreement dated the 12 day of MARCH, 2015 will remain in full force and effect.

IN WITNESS WHEREOF, the parties hereto have executed this Engagement Letter as of the date first above written.

CONTRACTOR



**Dave Perkins Contracting, Inc.**  
19745 Nowthen Blvd NW  
Coon Rapids, MN 55303-9655

By: \_\_\_\_\_

Its: \_\_\_\_\_

CITY OF COON RAPIDS

By: \_\_\_\_\_

Its: Mayor

By: \_\_\_\_\_

Its: City Manager

Recommended:

By: \_\_\_\_\_

Its: Public Works Director

Approved as to Form:

By: \_\_\_\_\_

Its: City Attorney

**EXHIBIT A**

**SERVICES TO BE PERFORMED BY CONSULTANT**

- ☐ Emergency watermain breaks
- ☐ Emergency sanitary sewer repairs
- ☐ Curb stop, gate valve, and hydrant repairs
- ☐ Drainage enhancements and flood protection projects
- ☐ Conduit installation
- ☐ Directional boring
- ☐ Equipment rentals (including laborers)
- ☐ Drilling services
- ☐ Miscellaneous specialty/technical services identified by the City
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_

**EXHIBIT B**  
**PROJECT SCHEDULE**

<b>Item</b>	<b>Date</b>
Construction commences	
Construction ends	

**EXHIBIT C****CONTRACTOR'S COMPENSATION FOR PROJECT**

<b>Item</b>	<b>Amount</b>	<b>Reimbursable Expenses</b>
<b>Total Project Costs</b>		

The above sums for services shall be Hourly – Time and Equipment, subject only to adjustments for a change in scope of services performed, agreed upon in writing by the City and the Contractor.





19745 Nowthen Blvd NW \* Ramsey, MN 55301 (763) 427-0109 Fax (763) 427-3806

## 2015 EQUIPMENT LIST & RATES WITH OPERATOR

EQUIPMENT TYPE	PER HOUR
314 Excavator	\$165.00
314 Excavator Winter Rates	\$190.00
325 Cat Excavator	\$185.00
325 Cat Excavator Winter Rates	\$200.00
330 Cat Excavator	\$210.00
Mini Excavator	\$145.00
WA350 Loader	\$125.00
D5XL Dozer	\$135.00
JCB Tractor Loader Backhoe	\$145.00
Ingersoll Rand SD700	\$105.00
Triaxle Dump Truck	\$100.00
Quad Dump Truck	\$105.00
Labor Per Hour	\$79.00
Lead Pipe Fitter With Truck & Tools	\$125.00
770A Motor Grader	\$125.00
Supervisor With Pick-up & Tools	\$125.00
Lowboy	\$125.00
Bobcat 770	\$135.00
90XT Case Skidsteer	\$95.00
90XT Case Skidsteer With Tracks	\$115.00
Large Plate Packer	\$105.00
Generator 4000 Watt	\$40.00
Trench Box Small	\$350.00
Dewatering Equipment Available	
Frost Ripping Per Job	
Overtime On All Equipment 1.5 Per Hour	
All Rates Normal Working Hours 6 Hours Minimum	
Mobilization & Demobilization	\$350.00 / \$450.00 out of area

Office Phone (763) 427-0109

24 Hour Call

Rene 612-363-6459

Dave 612-720-5755

Heidi 612-210-9772

# CITY OF COON RAPIDS

## LETTER OF ENGAGEMENT

Project Name: \_\_\_\_\_

Project No.: \_\_\_\_\_

This Letter of Engagement is entered into this 12 day of March, 2015 by and between the CITY OF COON RAPIDS, 11555 Robinson Drive, Coon Rapids, Minnesota 55433 (the "City") and

\_\_\_\_\_  
(Name, Address)

 **Dave Perkins Contracting, Inc.**  
**19745 Nowthen Blvd NW**  
**Nowthen, MN 55303-9655**

\_\_\_\_\_  
(the "Contractor").

### RECITALS

WHEREAS, the City and the Contractor entered into an Master Contracting Agreement (the "Agreement") on the 12 day of March, 2015; and

WHEREAS, this Agreement provides that the City will engage the Contractor from time to time to assist in providing construction services for projects and studies designated by the City and as described through separate Letters of Engagement; and

WHEREAS, the City wishes to retain Contractor to perform Service(s) to assist with the following described project:

Items as listed on the attached Exhibit A, on an as needed emergency basis.  
(the "Project"); and

WHEREAS, this Letter of Engagement outlines the Service(s) to be performed by the Contractor; the approved cost of the Project; and the Project schedule.

NOW, THEREFORE, in consideration of the mutual agreements herein contained and intending to be legally bound hereby, the City and the Contractor hereby agree as follows:

### ARTICLE ONE

#### SERVICES TO BE PERFORMED BY THE CONTRACTOR

The City hereby retains Contractor for the purposes of constructing the Service(s) described on Exhibit A attached hereto and made a part hereof.

### ARTICLE TWO

#### SCHEDULE OF PERFORMANCE

Given the general nature of the Service(s), the Contractor shall perform the Service(s) for the Project as determined by the City and Contractor. The schedule may be adjusted by mutual consent as the Service(s) proceed. The term limits established by the schedule and approved by the City shall not be exceeded by the Contractor or the City, except for a reasonable cause agreed to by the City.

**ARTICLE THREE**  
**CONTRACTOR'S COMPENSATION**

The City shall pay the Contractor for Service(s) furnished and the Contractor shall accept as full payment the sums described on the schedule of hourly rates and charges attached hereto and made a part hereof.

**ARTICLE FOUR**  
**CAPITALIZED TERMS**

Capitalized terms not otherwise defined herein have the meaning given them in this Agreement.

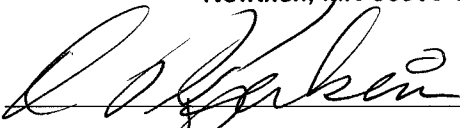

**ARTICLE FIVE**  
**CONTRACT TERM**

This Letter of Engagement will remain in effect until March 31, 2016, unless terminated by either party in accordance with the Master Contracting Agreement.

Subject to the terms and conditions of this Engagement Letter, all of the terms and conditions of the Master Contracting Agreement dated the 12 day of March, 2015 will remain in full force and effect.

IN WITNESS WHEREOF, the parties hereto have executed this Engagement Letter as of the date first above written.

CONTRACTOR  **Dave Perkins Contracting, Inc.**  
**19745 Nowthen Blvd NW**  
**Nowthen, MN 55303-9655**

By:   
Its: 

CITY OF COON RAPIDS

By: \_\_\_\_\_  
Its: Mayor

By: \_\_\_\_\_  
Its: City Manager

Recommended:

By: \_\_\_\_\_  
Its: Public Works Director

Approved as to Form:

By: \_\_\_\_\_  
Its: City Attorney

## EXHIBIT A

### SERVICES TO BE PERFORMED BY CONSULTANT

- ☒ Emergency watermain breaks
- ☒ Emergency sanitary sewer repairs
- ☒ Curb stop, gate valve, and hydrant repairs
- ☒ Drainage enhancements and flood protection projects
- ☐ Conduit installation
- ☐ Directional boring
- ☒ Equipment rentals (including laborers)
- ☒ Drilling services
- ☐ Miscellaneous specialty/technical services identified by the City
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_
- ☐ \_\_\_\_\_

**EXHIBIT B**  
**PROJECT SCHEDULE**

<b>Item</b>	<b>Date</b>
Construction commences	TBD, based upon nature of Service(s) provided
Construction ends	TBD, based upon nature of Service(s) provided

## **EXHIBIT C**

### **CONTRACTOR'S COMPENSATION FOR PROJECT**

The total compensation for the Service(s) provided shall be on an Hourly – Time and Equipment basis, subject only to adjustments for a change in scope of Service(s) performed, agreed upon in writing by the City and the Contractor.



19745 Nowthen Blvd NW \* Ramsey, MN 55301 (763) 427-0109 Fax (763) 427-3806

## 2015 EQUIPMENT LIST & RATES WITH OPERATOR

EQUIPMENT TYPE	PER HOUR
314 Excavator	\$165.00
314 Excavator Winter Rates	\$190.00
325 Cat Excavator	\$185.00
325 Cat Excavator Winter Rates	\$200.00
330 Cat Excavator	\$210.00
Mini Excavator	\$145.00
WA350 Loader	\$125.00
D5XL Dozer	\$135.00
JCB Tractor Loader Backhoe	\$145.00
Ingersoll Rand SD700	\$105.00
Triaxle Dump Truck	\$100.00
Quad Dump Truck	\$105.00
Labor Per Hour	\$79.00
Lead Pipe Fitter With Truck & Tools	\$125.00
770A Motor Grader	\$125.00
Supervisor With Pick-up & Tools	\$125.00
Lowboy	\$125.00
Bobcat 770	\$135.00
90XT Case Skidsteer	\$95.00
90XT Case Skidsteer With Tracks	\$115.00
Large Plate Packer	\$105.00
Generator 4000 Watt	\$40.00
Trench Box Small	\$350.00
Dewatering Equipment Available	
Frost Ripping Per Job	
Overtime On All Equipment 1.5 Per Hour	
All Rates Normal Working Hours 6 Hours Minimum	
Mobilization & Demobilization	\$350.00 / \$450.00 out of area
Office Phone (763) 427-0109	
24 Hour Call	
Rene 612-363-6459	
Dave 612-720-5755	
Heidi 612-210-9772	



**City Council Regular**

**16.**

**Meeting Date:** 04/07/2015

**Subject:** Consider Adopting Resolution 15-61 Year End Budget Appropriations

**Submitted For:** Kevin Vouk, Manager of Accounting/Treasurer

**From:** Dianne Nelson, Advanced Accounting Technician

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**INTRODUCTION**

Final 2014 General Fund budget reappropriations and carry-forwards from 2014 to 2015 are recommended.

**DISCUSSION**

Resolution 15-61 is attached for your consideration and includes the following items:

**Reallocation of appropriations in the 2014 General Fund budget**

After all transactions are completed each year, the General Fund budget is compared to year-to-date expenditures to ensure that each activity does not exceed budgeted amounts. Net adjustments equal to zero are being recommended. Budget reappropriations require a simple majority vote (4 members).

**Carryforward of items from the 2014 to 2015 budget**

Council is asked to consider carrying forward \$9,679 in the General Fund and \$1,154,257 for other funds into fiscal year 2015 for items not purchased or completed in 2014 as detailed in Resolution No. 15-61.

**RECOMMENDATION**

Staff recommends adoption of Resolution 15-61 Reallocating the 2014 General Fund Budget and Amending the 2014 and 2015 Budgets.

---

**Attachments**

**Res 15-61**

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## RESOLUTION 15-61

### RESOLUTION REALLOCATING THE 2014 GENERAL FUND BUDGET AND AMENDING THE 2014 AND 2015 BUDGETS

**WHEREAS**, Section 1-700 of the City Charter provides for adoption of an annual budget, and the subsequent re-appropriation of unencumbered balances for other purposes; and

**WHEREAS**, the City of Coon Rapids budget provides appropriations in a program format by activity; and

**WHEREAS**, the actual charges of personal hours and other expenses differs somewhat from budgetary estimates made over one year ago; and

**WHEREAS**, some 2014 budgeted items and services were not purchased in 2014.

**NOW, THEREFORE, BE IT RESOLVED**, that the City Council of the City of Coon Rapids hereby authorizes the re-appropriation of 2014 funds and amends the 2014 and 2015 budgets to include re-appropriation of 2014 budgeted funds to fiscal year 2015 as follows:

#### 2014 BUDGET ADJUSTMENTS (Reductions are indicated in parentheses.)

ACT #	DESCRIPTION	PERSONAL SERVICES	OTHER CHARGES & SERVICES	SUPPLIES	CAPITAL OUTLAY	OTHER DIS- BURSEMENTS	TOTAL
101	Legislative Control	(1,800)	(6,300)				(8,100)
102	Board & Commissions		(100)	100			0
103	Legal Services			(1,000)			(1,000)
104	Elections & Voter Registration	1,900	(3,000)	100			(1,000)
121	Maintenance of Official Records	(1,900)					(1,900)
122	Information	700	2,400				3,100
123	Management	800	(5,000)	(600)			(4,800)
124	Licensing	3,700	500	600			4,800
141	Human Resources	(3,500)	(40,000)				(43,500)
142	Fiscal Planning		(1,400)	200			(1,200)
143	Information Technology	100	(15,000)				(14,900)
144	General Revenue Collections		(1,000)				(1,000)
147	Property Appraisal		(1,500)				(1,500)
148	Purchasing		1,200	(1,200)			0
149	Payroll		200				200
201	Court Case/Preparation	(30,100)		(1,000)			(31,100)
202	Investigation/Non-Traffic		(4,000)	(4,000)	(4,100)		(12,100)
203	Preventative Patrol/Traffic		3,500	(12,000)			(8,500)
206	Community Education	30,100	(1,500)				28,600
207	Drug Enforcement & Education			(3,000)			(3,000)
208	Emergency Operation Planning		(2,500)				(2,500)
221	Fire Prevention		1,400	(500)			900
222	Fire Training		1,000				1,000
223	Fire Suppression	(1,900)	12,700	8,000			18,800
224	Rescue and First Aid		200	500			700
225	Fire Investigation		(500)				(500)
251	Employee Safety	1,900		300			2,200
302	Riverwind Recreational Center		300	100			400

ACT #	DESCRIPTION	PERSONAL SERVICES	OTHER CHARGES & SERVICES	SUPPLIES	CAPITAL OUTLAY	OTHER DIS- BURSEMENTS	TOTAL
305	Community Schools		(400)				(400)
320	Facilities Rental	(500)	300	200			0
323	Contract Grants		1,300				1,300
324	Multi-Purpose Senior Center	500	(300)	(200)			0
401	Planning		600				600
407	Neighborhood Reinvestment		(20,000)				(20,000)
421	Building Inspection		1,700				1,700
500	Snow Removal			(31,000)			(31,000)
501	Street Maintenance	(1,400)	7,000	2,300			7,900
503	Streetlight Maintenance		44,400				44,400
506	Garage/Vehicle Maintenance	200	2,800	500		7,100	10,600
507	Public Building Maintenance		22,600	2,200	4,100		28,900
509	Engineering Maintenance		(1,000)	100			(900)
510	Park Maintenance	(400)	5,600	3,700			8,900
511	Sidewalk Maintenance	1,200	200	6,800			8,200
514	Adult Softball Program		1,800	(500)			1,300
515	Indoor Skating		2,300	1,800	(1,500)	5,100	7,700
516	Outdoor Skating/Open	100	1,400	4,000			5,500
521	Tree Maintenance	300	600	300			1,200
	Total	-----	-----	-----	-----	-----	-----
		=====	=====	=====	=====	=====	=====

### 2014/2015 Budget Amendment

Re-appropriation of the following funds from the 2014 Budget to the 2015 Budget:

#### General Fund

Activity 144 – General Revenue Collections	\$500 Chairs
Activity 148 – Purchasing	\$1,000 Chairs
Activity 401 – Planning	\$1,000 Chairs
Activity 407 – Neighborhood Reinvestment	\$1,000 Chairs
Activity 421 – Building Inspection	\$2,000 Chairs
Activity 507 – Public Building Maintenance	<u>\$4,179</u> Card reader security locks at Public Works

\$9,679

#### Other Funds

Activity 716 – Public Communication	\$1,000 Chairs
Activity 741 – Recycling	\$44,200 Styrofoam processing machine
Activity 786 – Facilities Construction Fund	\$50,000 Study to reconfigure City Hall cubicles
Activity 786 – Facilities Construction Fund	\$94,000 Remodel restrooms
Activity 788 – Capital Equipment Fund	\$44,160 1-ton truck (Unit #114)
Activity 601 – Water Fund	\$257,125 Rehabilitation of wells
Activity 601 – Water Fund	\$3,147 RPZ back flow preventer valve
Activity 620 – Sewer Fund	<u>\$660,625</u> Sanitary sewer main lining

\$1,154,257

Adopted by the Coon Rapids City Council this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk



**City Council Regular**

**17.**

**Meeting Date:** 04/07/2015

**Subject:** Consider Adopting Resolution 15-62 Transfer of Funds

**From:** Kevin Vouk, Manager of  
Accounting/Treasurer

---

**INTRODUCTION**

The 2014 and 2015 budget documents include a number of transfers of funds that affect 2014. Resolution 15-62 requesting these transfers follows this memo.

**DISCUSSION**

The transfers which affect 2014 are as follows:

***General Fund to the Retirement Insurance Fund--\$540,472.*** This includes a \$500,000 budgeted transfer and \$40,472 of the unused balance of the \$150,000 budgeted transfer to the Compensated Absences Fund. When combined with other revenues and fund balance in the Retirement Insurance Fund, these amounts will fund the actuarial requirement for health insurance benefits for qualified retired police and firefighters.

***General Fund to the Facilities Construction Fund--\$1,100,000.*** City policy is to maintain an unassigned fund balance in the General Fund of at least 45% of the following year expenditures and transfers. Based on 2014 financial results and 2015 budgeted amounts, sufficient funds are available for this transfer while adhering to the fund balance policy.

***Coon Rapids MAF Program Fund to the Housing Program Fund--\$500,000.*** This is a transfer of funding previously approved by the City Council for the housing loan program.

***Lodging Tax Fund to the General Fund--\$12,928.*** The 3% lodging tax collected by hotels and motels in the City is recorded in the Lodging Tax Fund. The City remits 95% of the payments received to Minnesota Metro North Tourism and retains 5% which is transferred to the General Fund to defray administrative expenses.

***Senior Activity Fund to the General Fund--\$16,000.*** This transfer is from earnings on trips accounted for in the Senior Citizen Activity Fund and covers related administrative costs in the General Fund.

***Equipment Certificate Debt Service Fund to the Equipment Certificate Capital Projects Fund--\$304,463.*** Available funds in the Debt Service Fund are used toward the cost of fire trucks in the Capital Projects Fund instead of issuing equipment certificates.

***Revolving Construction Fund to the Street Reconstruction Fund--\$361,334.*** Prior year interest income of \$151,334 in the Revolving Construction Fund contributes to the sealcoating program costs. The remaining revenue over expenditures (\$210,000 based on an average) is used to fund part of the city's portion of street reconstruction costs.

***Insurance Reserve Fund to the General Fund--\$19,000.*** Operating income in the Insurance Reserve Fund is used to subsidize insurance costs in the General Fund.

***Insurance Reserve Fund to the Retirement Insurance Fund--\$93,967.*** Prior year interest income in the Insurance Reserve Fund contributes toward the funding of the actuarial liability of other post employment benefits.

***Various transfers as follows are needed so the funding or expenditures from certain construction projects or other items are in the proper fund or to close funds which are not longer needed:***

1992 Improvement Bonds Fund closed to the Special Assessments Bonds Fund--\$12,329

2008 Improvement Bonds Fund to the Special Assessment Bonds Fund--\$57,731

2014 Improvement Bonds Fund to the 2014 Bonded Projects Fund--\$255,991

***Various funds to the Compensated Absences Fund--\$113,763.*** Compensated absences (unused sick/vacation balances) for the non-enterprise funds are accounted for in the Compensated Absences Fund. To fully fund the liability of \$2,406,397 in this fund as of December 31, 2014, the following transfers are needed to cover the 2014 change in each fund's liability: \$109,529 from the General Fund, \$1,365 from the Public Communications Fund, and \$2,869 from the HRA Capital Projects Fund.

### **RECOMMENDATION**

Staff recommends the adoption of Resolution 15-62 Authorizing the Transfer of Funds.

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### **Attachments**

Res 15-62

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## RESOLUTION 15-62

### RESOLUTION AUTHORIZING TRANSFER OF FUNDS

**WHEREAS**, the 2014 and 2015 budgets include 2014 transfers between various funds; and

**WHEREAS**, funds are available to complete the transfers.

**NOW, THEREFORE, BE IT RESOLVED** to authorize the following 2014 transfers as indicated:

<u>Transfers from</u>	<u>Transfers to</u>	<u>Amount</u>
General Fund	Retirement Insurance Reserve Fund	\$540,472
General Fund	Facilities Construction Fund	1,100,000
Coon Rapids MAF Program Fund	Housing Program Fund	500,000
Lodging Tax Fund	General Fund	12,928
Senior Activity Fund	General Fund	16,000
Equipment Certificate Debt Service	Equipment Certificate Capital Projects	304,463
Revolving Construction Fund	Street Reconstruction Fund	361,334
Insurance Reserve Fund	General Fund	19,000
Insurance Reserve Fund	Retirement Insurance Fund	93,967
1992 Improvement Bonds Fund	Special Assessment Bonds Fund	12,329
2008 Improvement Bonds Fund	Special Assessment Bonds Fund	57,731
2014 Improvement Bonds Fund	2014 Bonded Projects Fund	255,991
General Fund	Compensated Absences Fund	109,529
Public Communications Fund	Compensated Absences Fund	1,365
HRA Capital Projects Fund	Compensated Absences Fund	2,869

Adopted by the Coon Rapids City Council this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk



**City Council Regular**

**18.**

**Meeting Date:** 04/07/2015

**Subject:** ~~PC-15-11, Consider Approval of Site Plan with Use Flexibility for Senior Housing, Crooked Lake Blvd and 109th, Coon Rapids Leased Housing Associates~~

**From:** Scott Harlicker, Planner

---

**INTRODUCTION**

The applicant is requesting site plan approval to construct a 166 unit senior housing apartment building with underground parking and use flexibility to allow a density of 35 units per acre..

**DISCUSSION**

**Background**

The applicant is proposing to construct a 166 unit, four story senior apartment building. The site plan includes 125 underground parking stalls and 51 surface parking spaces. In conjunction with this project, the applicant is also requesting preliminary plat approval to create the lot for the building. The master plan for PORT Campus Square identified this area as senior housing.

***Site Plan Criteria***

<b>Required Finding - Chapter 11-325 - General Requirements for All Site Plans</b>	<b>Staff Analysis and Comments</b>
(1) Be compatible with surrounding land uses	<b>OK</b> - The proposed site plan is compatible with the adjacent land uses.
(2) Preserve existing natural features whenever possible	<b>OK</b> - The existing trees along Crooked Lake Boulevard are being preserved. The tree preservation plan should be approved by the City Forester and installed and inspected prior to grading commencing.
(3) Achieve a safe and efficient circulation system	<b>OK</b> – The drive aisles are of sufficient width to allow for safe and efficient traffic circulation. The two access drives provide sufficient circulation for the project.
(4) Not place excessive traffic loads on local streets	<b>OK</b> – The majority of the traffic will enter the site via the existing driveway that serves the ice arena and Walgreens. There is a secondary access proposed from 109th Lane.
(5) Conform to the City’s plans for parks, streets, service drives, and walkways	<b>OK</b> – There is an existing sidewalk along Crooked Lake Boulevard. A trail will be installed along 109th Lane.
(6) Conform to the City’s Goals and Policies	<b>OK</b> – The project conforms with applicable City goals and policies.

(7) Achieve a maximum of safety, convenience, and amenities	<b>OK</b> – An internal sidewalk system connects the building entrances with the parking lot. Lighting is provided for the parking lot and driveway. The lights will have shields and cut offs so that the light is directed away from adjacent properties. The lighting plan complies with city ordinance.
(8) Show sufficient landscaping	<b>OK</b> – See discussion below.
(9) Not create detrimental disturbances to surrounding properties	<b>OK</b> – The project will not create disturbances which will be detrimental to the surrounding properties.
(10) Meet Title 11	<b>OK</b> - No variances are required.
(11) Show efforts to conserve energy whenever practical	<b>OK</b> - Landscaping is provided to provide shade for the parking lot. The project will utilize raingardens to handle stormwater.

### ***River Rapids Overlay Development Guidelines***

<b>RRO Standard (Chapter 11-2700)</b>	<b>Proposed</b>
<b>Two-Story Expression, 11-2711(1)</b> Pitched roofs, dormers, gables containing windows	<b>OK</b> – The building is a four stories high. It has a pitched roof with gables and dormers.
<b>Building Massing, 11-2711(2)</b> Staggered components, columns, recessed entry areas, avoid long, even facades	<b>OK</b> – The building is designed almost as a half circle so there are no long facades. The decks and columns also serve to break up the facade.
<b>Facades, 11-2711(3)</b> Articulated building facades.	<b>OK</b> – The different building materials, windows, and different angles provide articulation to break up the facades of the building.
<b>Varied Roof Shapes, 11-2711(4)</b> Variety (pitched roofs, dormers, chimneys, parapets, cornices)	<b>OK</b> – The building has a pitched roof that includes dormers and gables
<b>Focal Features, 11-2711(5)</b> Focal element (canopies, porticoes, overhangs, arches, recesses/projections, peaks/parapets over doors) on each building	<b>OK</b> – The entrance is the focal feature of the building.
<b>Canopies/Awnings/Entry Treatments, 11-2711(6)</b> Entries or focal features facing public streets	<b>OK</b> – The main entrance of the building is set off by a canopy, windows and a landscaped plaza.
<b>Building Materials, 11-2711(7)</b> High-quality materials (brick, natural stone, stucco, textured cast stone, fiber-cement siding; no steel panels, aluminum, vinyl, or fiberglass siding); 30% of facades facing a street must be brick or natural stone.	<b>OK</b> - The exterior of the building includes a variety of materials. Exterior materials include cultured stone, horizontal cement board siding and vertical board and batten siding. The variety of materials is carried through on all sides of the building. A trash enclosure is proposed near the entrance to the underground parking on the south end of the building. It should be constructed of similar material as the building.
<b>Windows and Doors, 11-2711(8)</b> Primary street level façade between 3 and 8 feet above grade must be 60% glass	<b>N/A</b> - this is a residential building, each unit has windows and patio doors.
<b>Outdoor Spaces, 11-2711(9)</b> Plazas, seating areas, outdoor furniture	<b>NO</b> – Seating should be provided in the plaza area in front of the main entrance.



<b>Parking, 11-2711(10)</b> Locate parking to rear or sides of buildings.	<b>OK</b> – See discussion below
<b>Pedestrian/Bicycle Access and Circulation, 11-2711(11)</b> Complete system from public ROW to buildings; minimum 5' wide sidewalks	<b>OK</b> – A sidewalk connection between this project and the ice arena is proposed as is a connection to the sidewalk along Crooked Lake Boulevard and to the 109th Lane trail.
<b>Urban Design Furniture/Amenities, 11-2711(12)</b> Compatible furniture/amenities (benches, trash receptacles, pedestrian-scale lighting, bicycle racks)	<b>NO</b> - See note in "Outdoor Spaces"
<b>Landscaping and Screening, 11-2711(13)</b>	<b>OK</b> - See discussion below.
<b>Lighting, 11-2711(14)</b> Accent lighting for building facades; compatible with applicable standards	<b>N/A</b> – No accent lighting is proposed for the buildings

### Parking

The parking requirement for senior housing is 1.2 spaces per unit; the parking requirement for a 166 unit apartment is 198 spaces. The site plan includes 176 parking spaces with proof of parking for 22. Of those 198 spaces, the code requires 132 off street common (.8/unit), 33 garage private (.2/unit) and 33 off street private (.2/unit). The applicant is proposing 125 garage spaces and 51 off street common spaces.

The applicant is proposing a more underground spaces than what is required because of the following reasons:

- Their senior residents prefer covered parking areas in lieu of surface spaces especially in the winter months.
- The exterior parking is largely for visitors, seniors will be parking under the building for greater security and ease of access.
- Taking advantage of the building footprint allows them to cover more spaces.
- More covered parking allows them to make use of more landscaped areas in lieu of surface parking.

### Landscaping

Many of the mature trees along Crooked Lake Boulevard are being preserved. There are several areas along the street that should be filled in with additional street trees. Six trees should be sufficient to fill in the gaps and create a line of street trees. Nine Patriot Elms are proposed along 109th Lane; an additional two trees should be planted to continue the line of trees to the existing ones that are being preserved. Perennials and shrubs will be planted along the foundation of the building and in the parking lot peninsulas and islands. A landscaped area, including overstory trees, evergreens and ornamental trees is proposed along the propertyline between this site and the ice arena property.

### Grading/Drainage

Attached is the Assistant City Engineer's comments. The outstanding engineering issues can be addressed prior to the issuance of a building building permit. They are not significant enough to warrant changes to the site plan. The applicant is proposing to save a significant number of trees along Crooked Lake Boulevard. The City Forester should approve the tree preservation plan and verify it's installation prior to grading commencing on site. Stormwater is being controlled via a series of rain gardens.

### Use Flexibility/Density

The applicant is proposing 166 units on 4.74 acres with a density of 35 units per acre. The maximum density allowed in the PORT district is 30 units per acre. The proposed site plan requires the granting of Use Flexibility for the 35 units per acre. Higher densities are allowed with the granting of Use Flexibility per Section 11-903.12(4).

The findings for granting Use Flexibility are as follows:

1. Advances Intent of Section : The density of 35 units per acre promotes a compact and vigorous development that takes advantage of the nearby commercial uses and the community events at the community ice arena and the nearby park. It includes a well organized vehicular and pedestrian circulation pattern. The

development is constructed of high quality materials and reflects and unique design. It is consistent with the goal of locating high density residential in the PORT District.

2. The modification is necessary to develop the property in an efficient well organized way. It allows the development of the property in an efficient well organized way.
3. The plan provides significant amenities and buffers to offset any potential harmful affects caused by the use. The plan provide site amenities such as walking trail/paths, connection to ice arena and preservation of existing trees and new landscaping to offset any potential harmful effects.

#### Planning Commission Meeting

At the Planning Commission meeting held on March 19th, no one spoke at the public hearing. The Commission had questions on the dumpster. The applicant stated that it will be stored inside, brought out the day of pick up then brought back inside. A concrete pad will be constructed to set the dumpster on while awaiting pick up. The Commission also asked for additional information regarding parking. The applicant explained that the parking configuration reflects the needs and desires of the residents. The applicant reviewed the exterior of the building and explained that the vertical siding will be changed to horizontal siding for maintenance reasons.

The Commission voted unanimously to recommend the City Council approve the proposed site plan.

#### **RECOMMENDATION**

In Planning Case 15-11, the Planning Commission recommended the City Council approve the site plan and the granting of Use Flexibility with the following conditions:

1. All engineering comments must be addressed.
2. An additional two street trees be planted along 109th lane and six trees along Crooked Lake Boulevard.
3. A tree preservation plan be approved by the City Forester and it's implementation verified prior to grading commencing on site.
4. Seating be provided on the plaza in front of the main entrance,
5. A concrete pad be constructed for the dumpster.
6. Use flexibility is granted based on the following findings:

1. Advances Intent of Section : The density of 35 units per acre promotes a compact and vigorous development that takes advantage of the nearby commercial uses and the community events at the community ice arena and the nearby park. It includes a well organized vehicular and pedestrian circulation pattern. The development is constructed of high quality materials and reflects and unique design. It is consistent with the goal of locating high density residential in the PORT District.
2. The modification is necessary to develop the property in an efficient well organized way. It allows the development of the property in an efficient well organized way.
3. The plan provides significant amenities and buffers to offset any potential harmful affects caused by the use. The plan provide site amenities such as walking trail/paths, connection to ice arena and preservation of existing trees and new landscaping to offset any potential harmful effects.

---

#### **Attachments**

Location Map

Site Plan

Grading Plan

Landscape Plan

Exterior Elevations

Exterior Elevations

Exterior Materials

Engineering Comments

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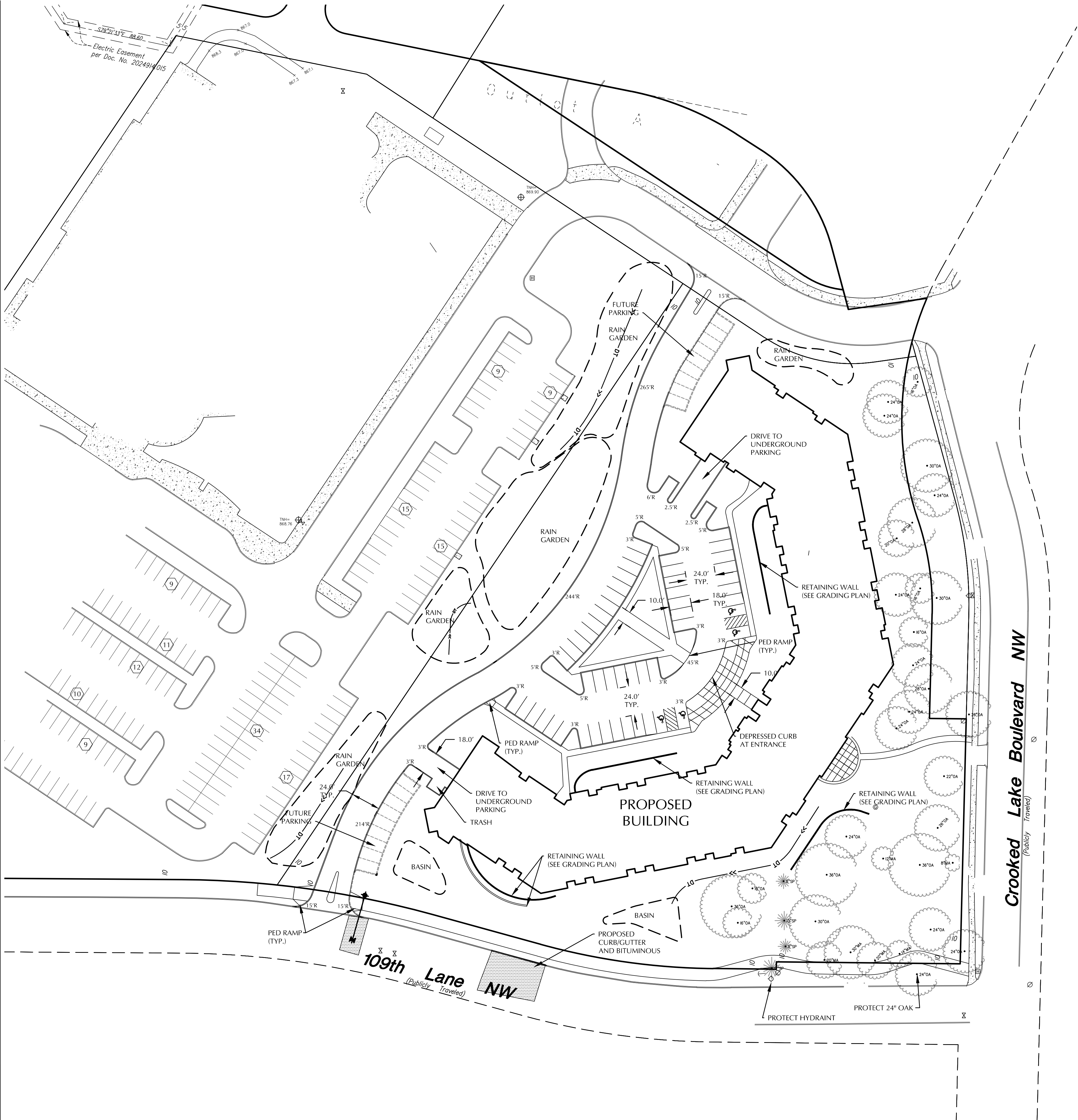


# Location Map





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TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

**SITE PLAN GENERAL NOTES**

1. PARKING SUMMARY  
UNDERGROUND REGULAR STALLS = 123  
UNDERGROUND HANDICAPPED STALLS = 2  
SURFACE REGULAR STALLS = 47  
SURFACE HANDICAPPED STALLS = 4
2. ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN PER SHEET C8-1 AND THE REQUIREMENTS OF THE CITY. SEE LANDSCAPE AND ARCHITECTURAL PLANS FOR ANY ADDITIONAL HARDSCAPE APPLICATIONS.
3. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPT. AND THE CONSTRUCTION ENGINEER SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE STREET RIGHT OF WAY (SIDEWALK, STREET OR DRIVEWAYS)
4. MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.
5. SEE CONTRACT SPECIFICATIONS FOR ANY REMOVAL DETAILS.
6. ANY SIGN OR FIXTURES REMOVED WITHIN THE RIGHT OF WAY OR AS PART OF THE SITE WORK SHALL BE REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH THE CITY REQUIREMENTS. THE CONTRACTOR SHALL PRESERVE AND MAINTAIN ANY EXISTING STREET LIGHTS AND TRAFFIC SIGNS PER THE REQUIREMENTS OF THE CITY.
7. CLEAR AND GRUB AND REMOVE ALL TREES, VEGETATION AND SITE DEBRIS PRIOR TO GRADING. ALL REMOVED MATERIAL SHALL BE HAULED FROM THE SITE DAILY. ALL CLEARING AND GRUBBING AND REMOVALS SHALL BE PERFORMED PER THE CONTRACT SPECIFICATIONS. EROSION CONTROL MEASURES SHALL BE IMMEDIATELY ESTABLISHED UPON REMOVAL. (SEE SHEET C3-1)
8. THE CONTRACTOR SHALL BE REQUIRED TO OBTAIN ALL PERMITS FROM THE CITY AS REQUIRED FOR ALL WORK WITH THE STREET AND PUBLIC RIGHT OF WAY.
9. A SIGNIFICANT PORTION OF SITE IMPROVEMENTS NOT SHOWN ON THIS SHEET ARE DESCRIBED AND PROVIDED IN FURTHER DETAIL ON THE ARCHITECTURAL AND LANDSCAPE PLANS. THIS INCLUDES LANDSCAPING, LIGHTING AND OTHER FIXTURES.
10. B61.2 CONCRETE CURB AND GUTTER SHALL BE INSTALLED AT THE EDGE OF ALL COMMON DRIVES AND PARKING LOTS.
11. CONSTRUCTION NOTES:  
A. PEDESTRIAN RAMPS SHALL BE PROVIDED AT THE LOCATIONS SHOWN AND AS SHOWN PER THE LANDSCAPE SITE PLANS.  
B. THE CONTRACTOR SHALL SAW-CUT BITUMINOUS AND CONCRETE PAVEMENTS AS REQUIRED PER THE SPECIFICATIONS. REMOVE EXISTING CURB AND GUTTER AND INSTALL B618 OR SIZE TO MATCH EXISTING CONCRETE CURB AND GUTTER.
12. SEE SHEETS C3-1 AND C4-1 FOR GRADING AND UTILITIES.
13. THE INTENT OF THE PROPOSED CONSTRUCTION IS TO PRESERVE AS MUCH OF THE EXISTING STREET PAVEMENT AS POSSIBLE, AND TO MILL AND OVERLAY. REMOVED PAVEMENT AREAS AND PATCHING SHALL BE INSTALLED PER PAVEMENT SECTION PROVIDED PER DETAIL SHEET.

**WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

Project Name:

**SENIOR HOUSING FACILITY**

Coon Rapids, MN

Owner/Developer:

DOMINIUM

Professional Services:

**LOUCKS ASSOCIATES**

Planning • Civil Engineering • Land Surveying  
Landscape Architecture • Environmental

7200 Hemlock Lane - Suite 300  
Maple Grove, MN 55369  
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C2-1.DWG/24x36

Submital:

02/09/15 DD SUBMITTAL  
03/06/15 SITE PLAN APPROVAL

Professional Signature:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Michael J. St. Martin - PE

24440  
License No. Date

Quality Control:

MJS DDL  
Project Lead: Drawn By:

MJS 03/06/15  
Checked By: Review Date:

Sheet Index:

C1-1 EXISTING CONDITIONS/DEMO PLAN  
C2-1 SITE PLAN  
C3-1 GRADING PLAN  
C3-2 SWPP PLAN  
C4-1 UTILITY PLAN  
C8-1 CIVIL DETAILS  
C8-2 CIVIL DETAILS

Sheet Title:

SITE PLAN

Project No.:

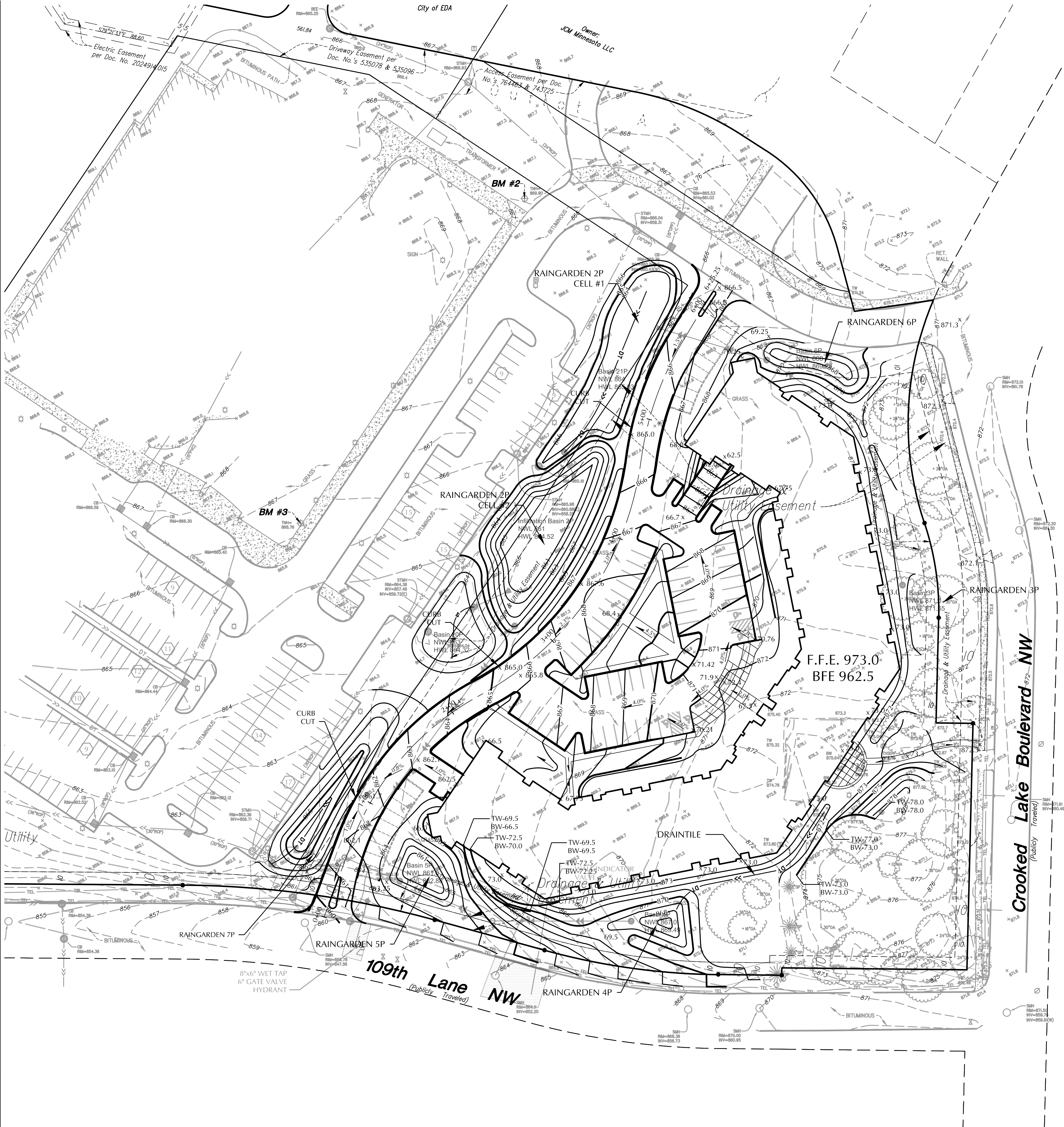
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Sheet No.:

C2-1



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**Gopher State One Call**  
TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

#### GRADING, DRAINAGE & EROSION CONTROL NOTES

1. THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF BUILDINGS, VESTIBULES, SLOPED PAVING, EXIT PORCHES, RAMPS, TRUCK DOCKS, ENTRY LOCATIONS AND LOCATIONS OF DOWNSPOUTS.
2. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 6 INCHES OF TOP SOIL AND SOD OR SEED. THESE AREAS SHALL BE WATERED BY THE CONTRACTOR UNTIL THE SOD OR SEED IS GROWING IN A HEALTHY MANNER.
3. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
4. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
5. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
6. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTORS PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
7. BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL A TEMPORARY ROCK ENTRANCE PAD AT ALL POINTS OF VEHICLE EXIT FROM THE PROJECT SITE. SAID ROCK ENTRANCE PAD SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE PROJECT. SEE DETAILS SHOWN ON SHEET C-8-2 OF THE PROJECT PLANS.
8. EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE ESTABLISHED AROUND THE ENTIRE SITE PERIMETER AND IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, CITY REQUIREMENTS AND THE DETAILS SHOWN ON SHEET C8-1 & C8-2 OF THE PROJECT PLANS.
9. ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED BY THE CITY.
10. SEE UTILITY PLAN FOR FURTHER DETAIL REGARDING THE STORM SEWER.

Project Name:

## SENIOR HOUSING FACILITY

Coon Rapids, MN

Owner/Developer:

DOMINIUM

Professional Services:

**LOUCKS ASSOCIATES**

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C3-1.DWG/24x36

Submital:

02/09/15 DD SUBMITTAL  
03/06/15 SITE PLAN APPROVAL

Professional Signature:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Michael J. St. Martin - PE

24440 License No. Date

Quality Control:

MJS DDL

Project Lead: MJS Drawn By: 03/06/15

Checked By: Review Date:

Sheet Index:

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C2-1 SITE PLAN  
C3-1 GRADING PLAN  
C3-2 SWPP PLAN  
C4-1 UTILITY PLAN  
C8-1 CIVIL DETAILS  
C8-2 CIVIL DETAILS

Sheet Title:

## GRADING AND DRAINAGE PLAN

Project No.:

14628.OA

Sheet No.:

C3-1

#### WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.







NORTH ELEVATION



EAST ELEVATION

## EXTERIOR ELEVATIONS - OPTION 1

***Coon Rapids Senior Apartments***

*Coon Rapids, MN*

*2-9-2015*



**BKV**  
GROUP





WEST ELEVATION



NORTH ELEVATION

EXTERIOR ELEVATIONS - OPTION 2

**Coon Rapids Senior Apartments**

Coon Rapids, MN

2-9-2015



CULTURED STONE



HORIZONTAL SIDING



HORIZONTAL SIDING



VERTICAL BOARD & BATTEN SIDING





TO: Scott Harlicker, Planner

FROM: Mark Hansen, Assistant City Engineer

SUBJECT: Dominion Senior Housing  
Proposed Site Development

DATE: March 11, 2015

The following comments are based on the Engineering Division's review of the revised site plan dated March 5, 2015 for a proposed senior housing facility to be located at 11000 Crooked Lake Boulevard. The site is located west of and adjacent to Crooked Lake Boulevard, and north of and adjacent to 109th Lane. Both Crooked Lake Boulevard and 109th Lane are City streets. The engineer/surveyor is Loucks Associates, and the developer/owner is Dominion.

#### Grading and Drainage Plan

- The City will need an easement for the portion of the proposed 10-foot wide bituminous trail located outside the public right of way along 109th Lane.
- Additional coordination with the City and the Coon Creek Watershed District will be necessary to complete the combination rain garden system design to be located along the western property line. This includes revising Raingarden 2P Cell #1 to include additional area behind the curb adjacent to the ice arena parking lot, as discussed at the City/Watershed coordination meeting held on March 9, 2015.
- An outlet pipe needs to be added at the end of the north driveway entrance to the underground garage.

#### Stormwater Pollution Prevention Plan

- Add the word "owner" after "The new..." in the last sentence of note #14.

#### Utility Plan

- The note for the post indicator valve shall read "Wall Post Indicator Valve" on the plans.
- Under general note #1 - update the City Engineers Association of Minnesota to the 2013 edition.
- General note #2 - sheets C8-1 to C8-2 do not currently depict utility service details. Update the plans as necessary to include the appropriate details.
- The existing 8" DIP watermain stub and valve located off of Crooked Lake Boulevard on the east side of the project shall be removed to the main. If any portion of this watermain pipe is to be left in place within the tree area, it shall be appropriately abandoned by blowing the pipe full with silica sand.

General Comments

- The applicant shall continue working with the Coon Creek Watershed District to obtain the necessary permits.

Cc: Tim Himmer, Public Works Director  
Grant Fernelius, Community Development Director  
Rebecca Haug, Coon Creek Watershed District



## **City Council Regular**

**19.**

**Meeting Date:** 04/07/2015

**Subject:** PC 15-10, Consider Approval of Preliminary and Final Plat, Robinwood Plat 6, 109th Lane and Crooked Lake Blvd., Coon Rapids Leased Housing Assoc.

**From:** Scott Harlicker, Planner

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### **INTRODUCTION**

The applicant is requesting preliminary and final plat approval to replat approximately 30 acres in PORT Campus Square into two lots and three outlots. Lot 1 is the Coon Rapids ice arena and Lot 2 is for a proposed senior housing building. The outlots are reserved for future development, a city park and access roads.

### **DISCUSSION**

#### **Background**

The applicant is proposing to replat nine parcels into two lots and three outlots. The majority of the site was originally platted and developed in the 1980's as a shopping center. Seven of the nine parcels contained the original buildings of that development; the other two parcels were adjacent single family residences. The plat includes 30 acres. One of the lots includes the ice arena the other is for a proposed senior housing apartment building. The outlots will be replatted at a future date as the area is redeveloped.

Lot 1 is 8.85 acres and Lot 2 is 4.74 acres. The plat also includes platting the east half of Floral Street as right-of-way, it is currently an easement. Outlot C is 60 feet wide to allow for future construction of a city street if needed. A portion of the right-of-way for Crooked Lake Boulevard will be platted as part of Lot 2. The remaining right-of-way will be 66 feet wide, consistent with the other the section of Crooked Lake Boulevard. The lots comply with the lot size, dimensional and setback requirements of PORT Campus Square. The outlots are subject to a separate platting process prior to redevelopment.

#### **Park Dedication**

Park dedication for the apartment building should be paid prior to releasing the plat for recording. The park dedication for the apartment building is \$225,760 (\$1,360 per unit X 166 units).

#### **Planning Commission Meeting**

At the Planning Commission meeting held on March 19th, no one spoke at the public hearing. The Commission unanimously recommended approval of the plat.

### **RECOMMENDATION**

In Planning Case 15-10, the Planning Commission recommends the City Council approve the preliminary and final plat with the following conditions:

1. All comments of the Assistant City Engineer be addressed.
2. That portion of the right-of-way of Crooked Lake Boulevard that is to included as part of Lot 2 must be formally vacated by the City prior to recording the plat.
3. Park dedication in the amount of \$225,760 (\$1,360 per unit X 166 units) must be paid prior to releasing the plat for recording.

---

### Attachments

Location Map

Preliminary Plat

Final Plat

Final Plat Resolution

---



# Location Map









ROBINWOOD PLAT 6

City of Coon Rapids  
County of Anoka  
Sec. 16, Twp. 31, Rng. 24

KNOW ALL PERSONS BY THESE PRESENTS: That City of Coon Rapids, a public body corporate and politic, fee owner, of the following described property situated in the County of Hennepin, State of Minnesota, to wit:

Lot 11 and 12, Robinwood, Anoka County, Minnesota

Lot 2, Block 1, Robinwood Plat 4, Anoka County, Minnesota

Outlot A, Lot 1, Block 2, Lot 1, Block 3, and that part of Lot 1, Block 1, Robinwood Plat 5 described as follows: Commencing at the most Easterly corner of said Lot 1; thence North 58 degrees 31 minutes 08 seconds West along the Northeasterly line of said Lot 1, 22.69 feet; thence continuing Westerly along said Northeasterly line on a curve concave to the Northeast, 207.31 feet; thence South 32 degrees 30 minutes 39 seconds West, radial to said curve, 176.89 feet; thence South 58 degrees 31 minutes 00 seconds East, 293.47 feet to the Easterly line of said Lot 1; thence Northerly along said Easterly line to the point of commencement, subject to easement of record, all in Robinwood Plat No. 5, Anoka County, Minnesota

Lots 1, 2 and 3, Block 1, Brody Associates Addition, Anoka County, Minnesota

Abstract Property

Has caused the same to be surveyed and platted as ROBINWOOD PLAT 6, and does hereby dedicate to the public for public use forever the public ways and the easements for drainage and utility purposes as shown on this plat.

In witness whereof said City of Coon Rapids, a public body corporate and politic has caused these presents to be signed by its proper officer this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_.

City of Coon Rapids, a public body corporate and politic

Name of owner, Owner title

State of \_\_\_\_\_

County of \_\_\_\_\_

This instrument was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_ by Name of owner, Owner title of City of Coon Rapids, a public body corporate and politic.

(Signature)

(Printed Name)

Notary Public \_\_\_\_\_ County, \_\_\_\_\_

My Commission Expires January 31, 20 \_\_\_\_

SURVEYORS CERTIFICATION

I, Henry D. Nelson, do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Dated this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_

Henry D. Nelson, Licensed Land Surveyor  
Minnesota License No. 17255

State of Minnesota  
County of Hennepin

This instrument was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_ by Henry D. Nelson, a Licensed Land Surveyor.

(Signature)

(Printed Name)

Notary Public \_\_\_\_\_ County, Minnesota

My Commission Expires January 31, 20 \_\_\_\_

COON RAPIDS, MINNESOTA

This plat of ROBINWOOD PLAT 6 was approved and accepted by the City Council of Coon Rapids, Minnesota, at a regular meeting thereof held this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_\_. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the City or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 505.03, Subd. 2.

CITY COUNCIL of Coon Rapids, MINNESOTA

By \_\_\_\_\_, Mayor By \_\_\_\_\_, Clerk

COON RAPIDS, MINNESOTA

I hereby certify that this plat of ROBINWOOD PLAT 6 was approved by the City Planning Commission of the City of Coon Rapids at a meeting thereof held this \_\_\_\_ day of \_\_\_\_\_, 201 \_\_\_\_\_. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the City or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minn. Statutes, Section 505.03, Subd. 2.

\_\_\_\_\_, Chairman

COUNTY SURVEYOR

I hereby certify that in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this \_\_\_\_ day of \_\_\_\_\_, 20 \_\_\_\_.

By \_\_\_\_\_

Larry D. Holium  
Anoka County Surveyor

COUNTY AUDITOR / TREASURER

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9, taxes payable in the year 20 \_\_\_\_ on the land hereinbefore described have been paid. Also, pursuant to Minnesota Statutes, Section 272.12, there are no delinquent taxes and transfer entered this \_\_\_\_ day of \_\_\_\_\_, 20 \_\_\_\_.

Property Tax Administrator

By \_\_\_\_\_, Deputy

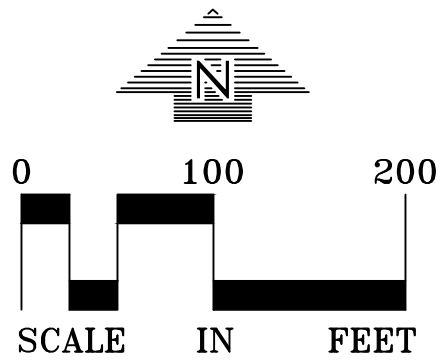
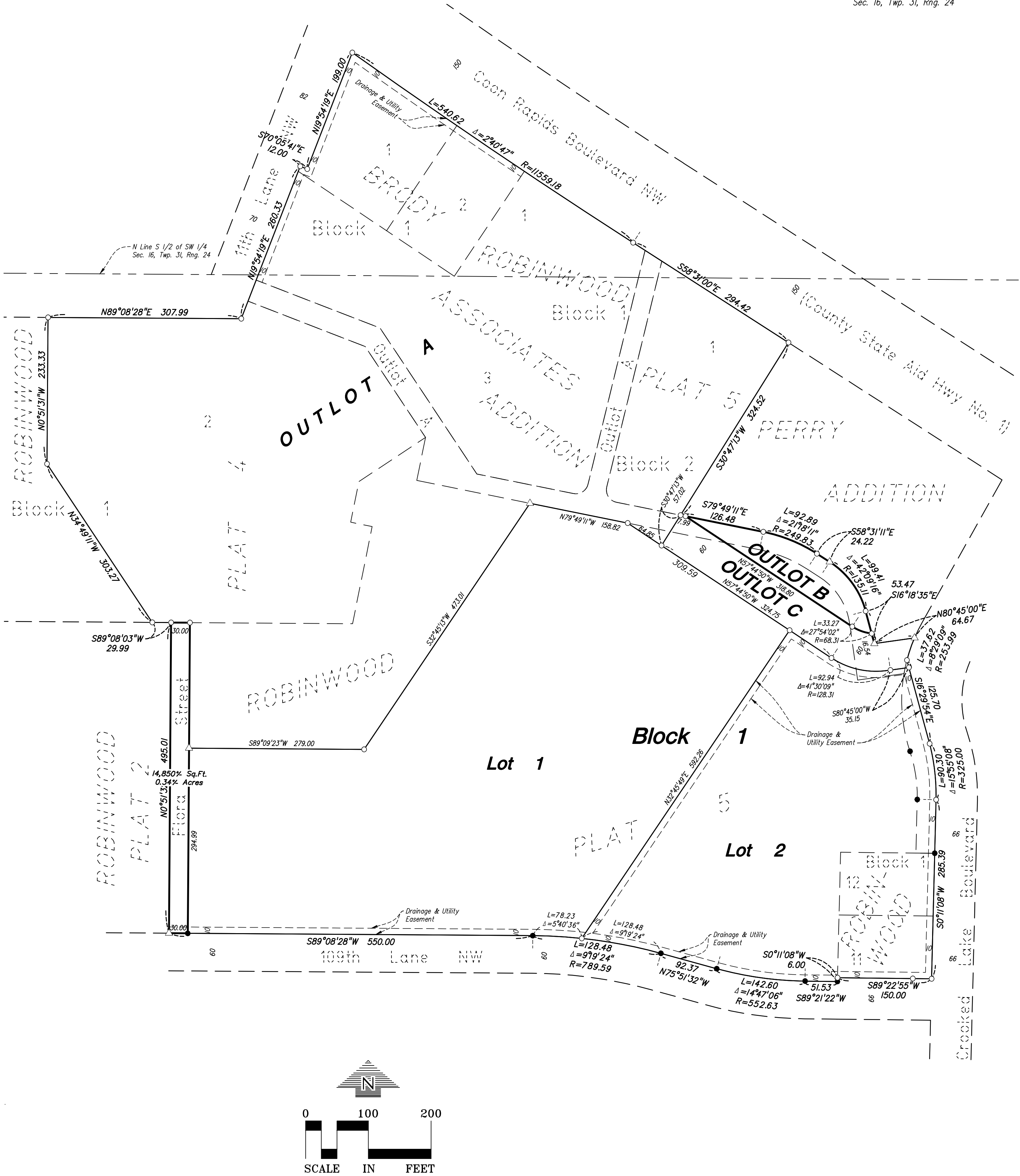
COUNTY RECORDER / REGISTRAR OF TITLES

County of Anoka, State of Minnesota

I hereby certify that this plat of ROBINWOOD PLAT 6 was filed in the office of the County Recorder / Registrar of Titles for public record on this \_\_\_\_ day of \_\_\_\_\_, 20 \_\_\_\_ at \_\_\_\_ o'clock \_\_\_\_ m. and was duly recorded in Book \_\_\_\_\_ Page \_\_\_\_\_, as Document Number \_\_\_\_\_.

County Recorder / Registrar of Titles

By \_\_\_\_\_, Deputy



- DENOTES 1/2 INCH X 14 INCH IRON MONUMENT SET, MARKED "RLS 17255"
- △ DENOTES SET NAIL
- DENOTES IRON MONUMENT FOUND

LOUCKS  
ASSOCIATES



**Resolution \_\_\_\_\_**

**A Resolution of the City of Coon Rapids Granting Approval of the Final Plat  
for Robinwood Plat 6, Planning Case No. PC 15-10**

**WHEREAS,** application has been made for approval of a final plat, pursuant to Section 11-1504.2 of the Coon Rapids Code of Ordinances, of the property legally described on attached Exhibit A and hereinafter referred to as “the Property”; and

**WHEREAS,** the Planning Commission on March 19, 2015 recommended approval of the preliminary plat of the Property;

**WHEREAS,** a preliminary plat of the Property was approved by the City Council on April 7, 2015; and

**WHEREAS,** the proposed final plat is consistent with the preliminary plat as approved by the City Council; and

**NOW, THEREFORE, BE IT RESOLVED**

1. It is hereby determined by the City Council for the City of Coon Rapids as follows:
  - A. The proposed subdivision is consistent with the Title 11, Chapter 1500 of the City Code of Ordinances and conforms with all of its requirements.
  - B. The proposed subdivision is consistent with all applicable general and specialized city, county, and regional plans including, but not limited to, the City’s Comprehensive Plan.
  - C. The physical characteristics of the site, including, but not limited to, topography, soils, vegetation, susceptibility to erosion and siltation, susceptibility to flooding, and drainage are suitable for the type and density of development and uses contemplated.
  - D. The proposed subdivision makes adequate provision for water supply, storm drainage, sewage transportation, parks and walkways, erosion control and all other services, facilities and improvements otherwise required herein.
  - E. The proposed subdivision will not cause substantial environmental damage.
  - F. The proposed subdivision will not have an undue or adverse impact on the reasonable development of neighboring land.

2. Approval of the final plat is hereby granted for the Property, subject to the conditions set forth in attached Exhibit B.
3. The officers of the City are hereby authorized, once the conditions set forth in Exhibit B are met and complied with, to sign the final plat for the Property and to issue a certified copy of this Resolution giving final approval of the survey.
4. The owner of the Property is authorized to record the final plat as required by law and shall file proof of said recording with the City. No building permits will be issued for the Property until the registered land survey is recorded.
5. This final plat approval may be rescinded 90 days from the date of this resolution if the plat is not recorded within that time.

Adopted this 7th day of April 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk

STATE OF MINNESOTA )  
COUNTY OF ANOKA     )ss.  
CITY OF COON RAPIDS )

CERTIFICATION OF CLERK

I, the undersigned, being the duly qualified and acting City Clerk of the City of Coon Rapids, Minnesota, hereby certify that I have carefully compared the attached and foregoing Resolution with the original thereof on file in my office and the same is a full, true and complete transcript therefrom, insofar as the same relates to Planning Case 15-11.

WITNESS hand officially as such City Clerk and the corporate seal of the City of Coon Rapids this \_\_\_\_ day of \_\_\_\_\_, 201\_.

---

Joan Lenzmeier, City Clerk

EXHIBIT A  
LEGAL DESCRIPTION

Lots 11 and 12, Robinwood, Anoka County, Minnesota

Lot 2, Block 1, Robinwood Plat 4, Anoka County, Minnesota

Lots 1, 2 and 3, Block 1, Brody Associates Addition, Anoka County, Minnesota

Outlot A, Lot 1, Block 2, Lot 1 Block 3, and that part of Lot 1, Block 1, Robinwood Plat 5 described as follows: Commencing at the most Easterly corner of said Lot 1; thence North 58 degrees 31 minutes 00 seconds West along the Noertheasterly line of said Lot 1, 22.69 feet; thence continuing Westerly along said Northeasterly line on a curve concave to the Northeast , 207.31 feet; thence South 32 degrees 30 minutes 39 seconds West, radial to said curve, 176.89 feet; thence South 58 degrees 31 minutes 00 seconds East, 293.47 feet to the Easterly line of said Lot 1; thence Northerly along said Easterly line to the point of commencement, subject to easements of record, all in Robinwood Plat 5, Anoka County, Minnesota

**EXHIBIT B**  
**CONDITIONS OF APPROVAL**

1. All comments of the Assistant City Engineer be addressed.
2. That portion of the right-of-way of Crooked Lake Boulevard that is to included as part of Lot 2 must be formally vacated by the City prior to recording the plat.
3. Park dedication in the amount of \$225,760 (\$1,360 per unit X 166 units) must be paid prior to releasing the plat for recording.



**City Council Regular**

**20.**

**Meeting Date:** 04/07/2015

**Subject:** Consider Adopting Resolution 15-59 Calling for Public Hearing on Street and Easement Vacation, 110xx Crooked Lake Blvd.

**From:** Matt Brown, Economic Development Coordinator

---

**INTRODUCTION**

The Council is asked to consider a resolution to set a public hearing on street and easement vacation along Crooked Lake Boulevard related to a senior housing development in Port Campus Square.

**DISCUSSION**

In Planning Case 15-10, the Council will consider a plat for an area south of Coon Rapids Boulevard and west of Crooked Lake Boulevard in Port Campus Square. This plat will create a lot for a 166-unit senior housing development. The EDA will consider a Purchase and Redevelopment Agreement related to this development on April 7 after the Council meeting. This proposed lot includes a strip of existing street right-of-way and associated easements along the west side of Crooked Lake Boulevard. City staff has determined that the right-of-way is unnecessarily wide at this location and it is appropriate to vacate an approximately 30-foot strip and include it in the development parcel. The right-of-way currently narrows to a more standard 66 feet south of this location. A vacation petition may be initiated by the City Council upon adoption of a resolution by a 4/5 majority. Resolution 15-59 calls for a public hearing on May 5, 2015 for the Council to consider this vacation.

**RECOMMENDATION**

Adopt Resolution 15-59, which calls for a public hearing on May 5, 2015 for vacation of a street and drainage and utility easements along a portion of Crooked Lake Boulevard.

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**Attachments**

Resolution 15-59

Street Vacation Location

Easement Vacation Location

---

**RESOLUTION NO. 15-59**

**RESOLUTION TO CONSIDER AND SET A  
PUBLIC HEARING ON A STREET, DRAINAGE AND UTILITY EASEMENT**

**WHEREAS**, The City Council pursuant to Minnesota Statute §412.851 desires to consider the vacation of a portion of the street for Crooked Lake Boulevard and the drainage and utility easement legally described as:

Street Vacation Description

That part of Crooked Lake Boulevard as dedicated in the plat of Robinwood Plat 3, Anoka County, Minnesota, according to the recorded plat thereof, lying westerly of the following described line:

Beginning at the southeast corner of said Crooked Lake Boulevard; thence North 0 degrees 11 minutes 08 seconds East, on an assumed bearing along the easterly line of said Crooked Lake Boulevard, a distance of 84.96 feet to a point of curve in said easterly line; thence continuing northerly, along said easterly line, a distance of 88.48 feet to a point of curve in said easterly line; thence North 17 degrees 39 minutes 57 seconds West, to the westerly line of said Crooked Lake Boulevard, and said line there terminating.

Drainage and Utility Easement Description

The most easterly 10.00 feet of Lot 1, Block 3 and the most easterly 10.00 feet of Outlot A as dedicated in the plat of Robinwood Plat 5, according to the recorded plat thereof, Anoka, County, Minnesota.

**NOW, THEREFORE, BE IT RESOLVED** Council will consider the vacation of such street, drainage and utility easement and a public hearing shall be held on such proposed vacation on the 5<sup>th</sup> day of May, 2015 at City Hall.

Adopted by the Coon Rapids City Council this 7th day of April, 2015.

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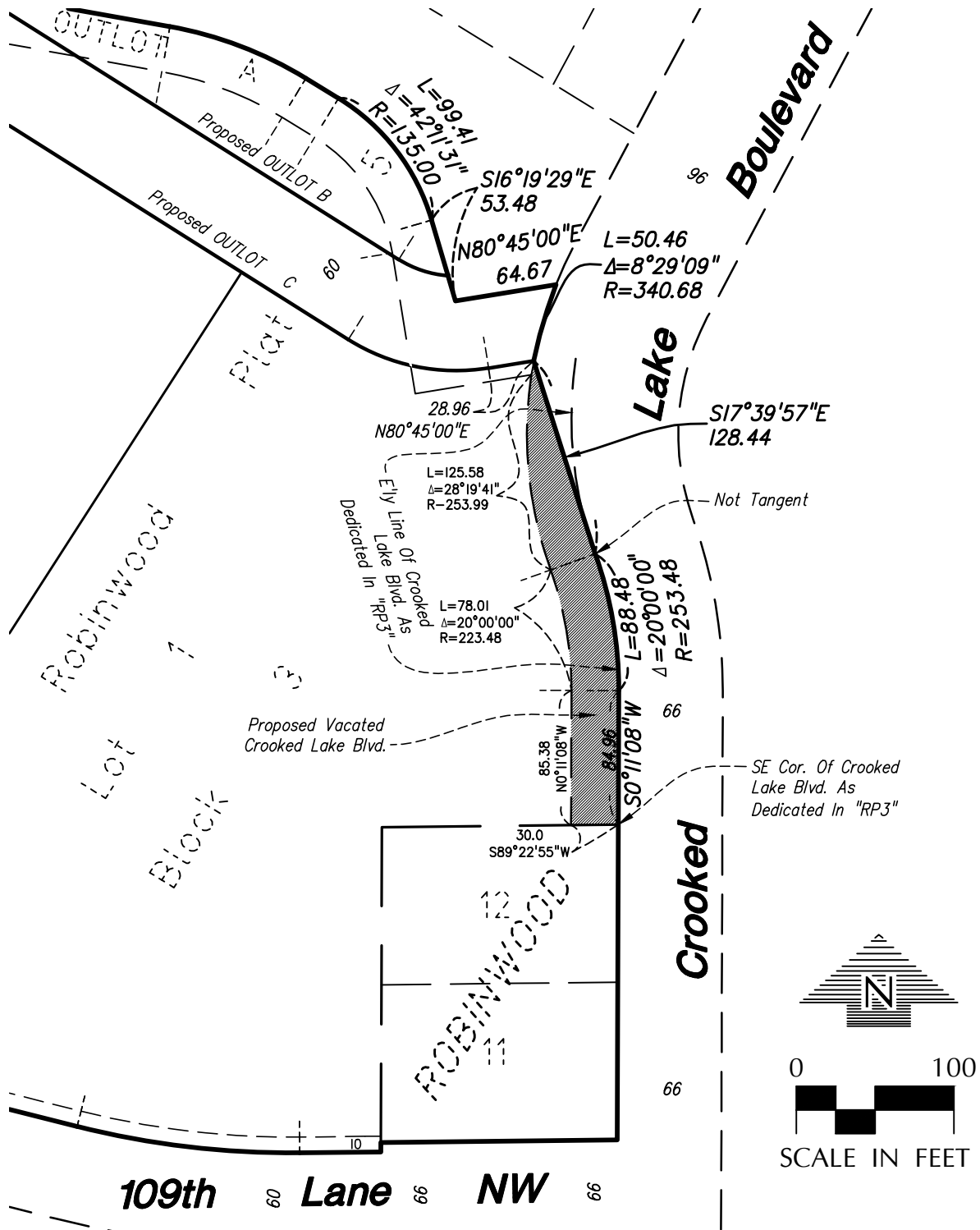
Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk

# EXHIBIT A



**LOUCKS  
ASSOCIATES**

Planning • Civil Engineering • Land Surveying  
Landscape Architecture • Environmental

7200 Hemlock Lane, Suite 300  
Minneapolis, Minnesota 55369-5592  
Telephone: (763)424-5505 Fax: (763)424-5822  
Web Page: [www.LoucksAssociates.com](http://www.LoucksAssociates.com)

*Henry D. Nelson*  
Henry D. Nelson - PLS

17255  
License No.

3-11-15  
Date



# Crooked



0 100

SCALE IN FEET

© 2005



**City Council Regular**

**21.**

**Meeting Date:** 04/07/2015

**Subject:** Consider Resolution 15-60 Establishing Tax Increment Financing Plan for District 1-31

**From:** Matt Brown, Economic Development  
Coordinator

---

**INTRODUCTION**

The Council is asked to consider a Tax Increment Financing (TIF) Plan for TIF District 1-31 related to a senior housing development in Port Campus Square.

**DISCUSSION**

Following the April 7 Council meeting, the EDA will consider a Purchase and Redevelopment Agreement with Dominion, Inc. for a 166-unit senior housing development in Port Campus Square east of the Coon Rapids Ice Center. The Council will consider a site plan and plat for this project at its April 7 meeting. The proposed financial assistance for this project includes establishing a new housing TIF district that would generate \$1,550,000. Dominion would receive incremental property taxes on a pay-as-you-go basis for approximately 25 years. These funds would be used to repay a note and finance the project. The proposed project will meet requirements of state statutes related to use of TIF for affordable housing. In order to establish the new TIF district, the Council must hold a public hearing, approve the TIF Plan for the new district, and approve a modification to the Redevelopment Plan for TIF Project Area No. 1.

**RECOMMENDATION**

Staff recommends that the Council:

1. Conduct a public hearing.
2. Adopt Resolution 15-60 modifying the Redevelopment Plan for Project Area No. 1 and establishing TIF District 1-31.

---

**Attachments**

Location Map

Resolution 15-60

TIF Plan, District 1-31

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## **RESOLUTION NO. 15-60**

### **RESOLUTION ADOPTING A MODIFICATION TO THE REDEVELOPMENT PLAN FOR REDEVELOPMENT PROJECT AREA NO. 1 AND ESTABLISHING TAX INCREMENT FINANCING DISTRICT NO. 1-31 THEREIN AND ADOPTING A TAX INCREMENT FINANCING PLAN THEREFOR.**

BE IT RESOLVED by the City Council (the "Council") of the City of Coon Rapids, Minnesota (the "City"), as follows:

#### Section 1. Recitals.

1.01. The Board of Commissioners of the Economic Development Authority in and for the City of Coon Rapids (the "EDA") has heretofore established Redevelopment Project Area No. 1 and adopted a Redevelopment Plan therefor. It has been proposed by the EDA and the City that the City adopt a Modification to the Redevelopment Plan for Redevelopment Project Area No. 1 (the "Redevelopment Plan Modification") and establish Tax Increment Financing District No. 1-31 (the "District") therein and adopt a Tax Increment Financing Plan (the "TIF Plan") therefor (the Redevelopment Plan Modification and the TIF Plan are referred to collectively herein as the "Plans"); all pursuant to and in conformity with applicable law, including Minnesota Statutes, Sections 469.090 to 469.1082 and Sections 469.174 to 469.1794, all inclusive, as amended, (the "Act") all as reflected in the Plans, and presented for the Council's consideration.

1.02. The EDA and City have investigated the facts relating to the Plans and have caused the Plans to be prepared.

1.03. The EDA and City have performed all actions required by law to be performed prior to the establishment of the District and the adoption and approval of the proposed Plans, including, but not limited to, notification of Anoka County and Independent School District No. 11 having taxing jurisdiction over the property to be included in the District, a review of and written comment on the Plans by the City Planning Commission, approval of the Plans by the EDA on April 7, 2015, and the holding of a public hearing upon published notice as required by law.

1.04. Certain written reports (the "Reports") relating to the Plans and to the activities contemplated therein have heretofore been prepared by staff and consultants and submitted to the Council and/or made a part of the City files and proceedings on the Plans. The Reports include data, information and/or substantiation constituting or relating to the basis for the other findings and determinations made in this resolution. The Council hereby confirms, ratifies and adopts the Reports, which are hereby incorporated into and made as fully a part of this resolution to the same extent as if set forth in full herein.

1.05. The City is not modifying the boundaries of Redevelopment Project Area No. 1, but is however, modifying the Redevelopment Plan therefor.

Section 2.     Findings for the Adoption and Approval of the Redevelopment Plan Modification.

2.01. The Council approves the Redevelopment Plan Modification, and specifically finds that: (a) the land within the Project area would not be available for redevelopment without the financial aid to be sought under this Redevelopment Plan; (b) the Redevelopment Plan, as modified, will afford maximum opportunity, consistent with the needs of the City as a whole, for the development of the Project by private enterprise; and (c) that the Redevelopment Plan, as modified, conforms to the general plan for the development of the City as a whole.

Section 3.     Findings for the Establishment of Tax Increment Financing District No. 1-31.

3.01. The Council hereby finds that Tax Increment Financing District No. 1-31 is in the public interest and is a "housing district" under Minnesota Statutes, Section 469.174, Subd. 11 of the Act.

3.02. The Council further finds that the proposed development would not occur solely through private investment within the reasonably foreseeable future, that the Plans conform to the general plan for the development or redevelopment of the City as a whole; and that the Plans will afford maximum opportunity consistent with the sound needs of the City as a whole, for the development or redevelopment of the District by private enterprise.

3.03. The Council further finds, declares and determines that the City made the above findings stated in this Section and has set forth the reasons and supporting facts for each determination in writing, attached hereto as Exhibit A.

Section 4.     Public Purpose.

4.01. The adoption of the Plans conforms in all respects to the requirements of the Act and will help fulfill a need to develop an area of the City which is already built up, to provide housing opportunities, to improve the tax base and to improve the general economy of the State and thereby serves a public purpose. For the reasons described in Exhibit A, the City believes these benefits directly derive from the tax increment assistance provided under the TIF Plan. A private developer will receive only the assistance needed to make this development financially feasible. As such, any private benefits received by a developer are incidental and do not outweigh the primary public benefits.

Section 5.     Approval and Adoption of the Plans.

5.01. The Plans, as presented to the Council on this date, including without limitation the findings and statements of objectives contained therein, are hereby approved, ratified, established, and adopted and shall be placed on file in the office of the Community Development Director.

5.02. The staff of the City, the City's advisors and legal counsel are authorized and directed to proceed with the implementation of the Plans and to negotiate, draft, prepare and

present to this Council for its consideration all further plans, resolutions, documents and contracts necessary for this purpose.

5.03 The Auditor of Anoka County is requested to certify the original net tax capacity of the District, as described in the Plans, and to certify in each year thereafter the amount by which the original net tax capacity has increased or decreased; and the Coon Rapids Economic Development Authority is authorized and directed to forthwith transmit this request to the County Auditor in such form and content as the Auditor may specify, together with a list of all properties within the District, for which building permits have been issued during the 18 months immediately preceding the adoption of this resolution.

5.04. The Community Development Director is further authorized and directed to file a copy of the Plans with the Commissioner of the Minnesota Department of Revenue and the Office of the State Auditor pursuant to Minnesota Statutes 469.175, Subd. 4a.

Adopted by the Coon Rapids City Council this 7th day of April, 2015.

---

Jerry Koch, Mayor

ATTEST:

---

Joan Lenzmeier, City Clerk



EXHIBIT A  
RESOLUTION NO. 15-60

The reasons and facts supporting the findings for the adoption of the Tax Increment Financing Plan for Tax Increment Financing District No. 1-31, as required pursuant to Minnesota Statutes, Section 469.175, Subdivision 3 are as follows:

1. *Finding that Tax Increment Financing District No. 1-31 is a housing district as defined in M.S., Section 469.174, Subd. 11.*

TIF District No. 1-31 consists of 3 parcels. The development will consist of senior rental apartment housing. All or a portion of which will receive tax increment assistance and will meet income restrictions described in *M.S. 469.1761*. At least 40 percent of the units/homes receiving assistance will have incomes at or below 60 percent of area median income. Appendix E of the TIF Plan contains background for the above finding.

2. *Finding that the proposed development, in the opinion of the City Council, would not reasonably be expected to occur solely through private investment within the reasonably foreseeable future.*

*The proposed development, in the opinion of the City, would not reasonably be expected to occur solely through private investment within the reasonably foreseeable future:* This finding is supported by the fact that the development proposed in this plan provides for housing that meets the City's objectives for development and redevelopment. Due to the high cost of building affordable new housing in the City and the cost of financing the proposed public improvements, this project is feasible only through assistance, in part, from tax increment financing. The developer was asked for and provided an application and a proforma as justification that the developer would not have gone forward without tax increment assistance.

*The increased market value of the site that could reasonably be expected to occur without the use of tax increment financing would be less than the increase in market value estimated to result from the proposed development after subtracting the present value of the projected tax increments for the maximum duration of the TIF District permitted by the TIF Plan:* The City reasonably determines that no other development of similar scope is anticipated on this site without substantially similar assistance being provided to the development.

3. *Finding that the TIF Plan for Tax Increment Financing District No. 1-31 conforms to the general plan for the development or redevelopment of the municipality as a whole.*

The Planning Commission reviewed the TIF Plan and found that the TIF Plan conforms to the general development plan of the City.

4. *Finding that the TIF Plan for Tax Increment Financing District No. 1-31 will afford maximum opportunity, consistent with the sound needs of the City as a whole, for the development or redevelopment of Redevelopment Project Area No. 1 by private enterprise.*

Through the implementation of the TIF Plan, the EDA or City will provide an impetus for residential development, which is desirable or necessary for increased population and an increased need for life-cycle housing within the City.





*As of March 31, 2015  
Draft for Public Hearing*

**Modification to the Redevelopment Plan  
for Redevelopment Project Area No. 1**

**and the**

**Tax Increment Financing Plan**

**for the establishment of**

**Tax Increment Financing District No. 1-31  
(a housing district)**

**within**

**Redevelopment Project Area No. 1**

Economic Development Authority in and for the City of Coon Rapids  
City of Coon Rapids  
Anoka County  
State of Minnesota

Public Hearing: April 7, 2015  
Adopted:



Prepared by: EHLERS & ASSOCIATES, INC.  
3060 Centre Pointe Drive, Roseville, Minnesota 55113-1105  
651-697-8500 fax: 651-697-8555 [www.ehlers-inc.com](http://www.ehlers-inc.com)

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***Section 1 - Modification to the Redevelopment Plan  
for Redevelopment Project Area No. 1***

**Foreword**

The following text represents a Modification to the Redevelopment Plan for Redevelopment Project Area No. 1. This modification represents a continuation of the goals and objectives set forth in the Redevelopment Plan for Redevelopment Project Area No. 1. Generally, the substantive changes include the establishment of Tax Increment Financing District No. 1-31.

For further information, a review of the Redevelopment Plan for Redevelopment Project Area No. 1 is recommended. It is available from the Community Development Specialist at the City of Coon Rapids. Other relevant information is contained in the Tax Increment Financing Plans for the Tax Increment Financing Districts located within Redevelopment Project Area No. 1.

## **Section 2 - Tax Increment Financing Plan for Tax Increment Financing District No. 1-31**

### **Subsection 2-1. Foreword**

The Economic Development Authority in and for the City of Coon Rapids (the "EDA"), the City of Coon Rapids (the "City"), staff and consultants have prepared the following information to expedite the establishment of Tax Increment Financing District No. 1-31 (the "District"), a housing tax increment financing district, located in Redevelopment Project Area No. 1.

### **Subsection 2-2. Statutory Authority**

Within the City, there exist areas where public involvement is necessary to cause development or redevelopment to occur. To this end, the EDA and City have certain statutory powers pursuant to *Minnesota Statutes ("M.S.")*, Sections 469.090 to 469.1082, inclusive, as amended, and *M.S.*, Sections 469.174 to 469.1794, inclusive, as amended (the "Tax Increment Financing Act" or "TIF Act"), to assist in financing public costs related to this project.

This section contains the Tax Increment Financing Plan (the "TIF Plan") for the District. Other relevant information is contained in the Modification to the Redevelopment Plan for Redevelopment Project Area No. 1.

### **Subsection 2-3. Statement of Objectives**

The District currently consists of three parcels of land and adjacent and internal rights-of-way. The District is being created to facilitate construction of 166 units of affordable senior apartments in the City. Please see Appendix A for further District information. The city has approved a preliminary Term Sheet with Dominium and will enter into an agreement with the company as the developer. This TIF Plan is expected to achieve many of the objectives outlined in the Redevelopment Plan for Redevelopment Project Area No. 1.

The activities contemplated in the Modification to the Redevelopment Plan and the TIF Plan do not preclude the undertaking of other qualified development or redevelopment activities. These activities are anticipated to occur over the life of Redevelopment Project Area No. 1 and the District.

### **Subsection 2-4. Redevelopment Plan Overview**

1. Property to be Acquired - The EDA or City currently owns two parcels of property within the District. The remaining property located within the District may be acquired by the developer, EDA or City and is further described in this TIF Plan.
2. Relocation - Relocation services, to the extent required by law, are available pursuant to *M.S.*, Chapter 117 and other relevant state and federal laws.
3. Upon approval of a developer's plan relating to the project and completion of the necessary legal requirements, the EDA or City may sell to a developer selected properties that it owns or may acquire within the District or may lease land or facilities to a developer.
4. The EDA or City may perform or provide for some or all necessary acquisition, construction, relocation, demolition, and required utilities and public street work within the District.

## **Subsection 2-5. Description of Property in the District and Property To Be Acquired**

The District encompasses all property and adjacent rights-of-way and abutting roadways identified by the parcels listed in Appendix C of this TIF Plan. Please also see the map in Appendix B for further information on the location of the District.

The EDA or City may acquire any parcel within the District including interior and adjacent street rights of way. Any properties identified for acquisition will be acquired by the EDA or City only in order to accomplish one or more of the following: storm sewer improvements; provide land for needed public streets, utilities and facilities; carry out land acquisition, site improvements, clearance and/or development to accomplish the uses and objectives set forth in this plan. The EDA or City may acquire property by gift, dedication, condemnation or direct purchase from willing sellers in order to achieve the objectives of this TIF Plan. Such acquisitions will be undertaken only when there is assurance of funding to finance the acquisition and related costs.

## **Subsection 2-6. Classification of the District**

The EDA and City, in determining the need to create a tax increment financing district in accordance with *M.S., Sections 469.174 to 469.1794*, as amended, inclusive, find that the District, to be established, is a housing district pursuant to *M.S., Section 469.174, Subd. 11* and *M.S., Section 469.1761* as defined below:

*M.S., Section 469.174, Subd.11:*

*"Housing district" means a type of tax increment financing district which consists of a project, or a portion of a project, intended for occupancy, in part, by persons or families of low and moderate income, as defined in chapter 462A, Title II of the National Housing Act of 1934, the National Housing Act of 1959, the United States Housing Act of 1937, as amended, Title V of the Housing Act of 1949, as amended, any other similar present or future federal, state, or municipal legislation, or the regulations promulgated under any of those acts, and that satisfies the requirements of M.S., Section 469.1761. Housing project means a project, or portion of a project, that meets all the qualifications of a housing district under this subdivision, whether or not actually established as a housing district.*

*M.S., Section 469.1761:*

*Subd. 1. Requirement imposed.*

*(a) In order for a tax increment financing district to qualify as a housing district:*

*(1) the income limitations provided in this section must be satisfied; and*

*(2) no more than 20 percent of the square footage of buildings that receive assistance from tax increments may consist of commercial, retail, or other nonresidential uses.*

*(b) The requirements imposed by this section apply to property receiving assistance financed with tax increments, including interest reduction, land transfers at less than the Authority's cost of acquisition, utility service or connections, roads, parking facilities, or other subsidies. The provisions of this section do not apply to districts located within a targeted area as defined in Section 462C.02 Subd 9, clause (e).*

*(c) For purposes of the requirements of paragraph (a), the authority may elect to treat an addition*

*to an existing structure as a separate building if:*

- (1) construction of the addition begins more than three years after construction of the existing structure was completed; and*
- (2) for an addition that does not meet the requirements of paragraph (a), clause (2), if it is treated as a separate building, the addition was not contemplated by the tax increment financing plan which includes the existing structure.*

*Subd. 2. Owner occupied housing.*

*For owner occupied residential property, 95 percent of the housing units must be initially purchased and occupied by individuals whose family income is less than or equal to the income requirements for qualified mortgage bond projects under section 143(f) of the Internal Revenue Code.*

*Subd. 3. Rental property.*

*For residential rental property, the property must satisfy the income requirements for a qualified residential rental project as defined in section 142(d) of the Internal Revenue Code. The requirements of this subdivision apply for the duration of the tax increment financing district.*

*Subd. 4. Noncompliance; enforcement.*

*Failure to comply with the requirements of this section is subject to M.S., Section 469.1771.*

In meeting the statutory criteria the EDA and City rely on the following facts and findings:

- The District consists of three parcels.
- The development will consist of 166 units of multi-family rental housing.
- 40% of the units will be occupied by person with incomes less than 60% of median income.

Pursuant to *M.S., Section 469.176, Subd. 7*, the District does not contain any parcel or part of a parcel that qualified under the provisions of *M.S., Sections 273.111 or 273.112 or Chapter 473H* for taxes payable in any of the five calendar years before the filing of the request for certification of the District.

## **Subsection 2-7. Duration and First Year of Tax Increment of the District**

Pursuant to *M.S., Section 469.175, Subd. 1, and Section 469.176, Subd. 1*, the duration and first year of tax increment of the District must be indicated within the TIF Plan. Pursuant to *M.S., Section 469.176, Subd. 1b.*, the duration of the District will be 25 years after receipt of the first increment by the EDA or City (a total of 26 years of tax increment). The EDA or City elects to receive the first tax increment in 2017, which is no later than four years following the year of approval of the District. Thus, it is estimated that the District, including any modifications of the TIF Plan for subsequent phases or other changes, would terminate after 2042, or when the TIF Plan is satisfied. The EDA or City reserves the right to decertify the District prior to the legally required date.

## **Subsection 2-8. Original Tax Capacity, Tax Rate and Estimated Captured Net Tax Capacity Value/Increment and Notification of Prior Planned Improvements**

Pursuant to *M.S., Section 469.174, Subd. 7 and M.S., Section 469.177, Subd. 1*, the Original Net Tax Capacity (ONTC) as certified for the District will be based on the market values placed on the property by the assessor in 2015 for taxes payable 2016, assuming the request for certification is made before June 30, 2016.

Pursuant to *M.S., Section 469.177, Subds. 1 and 2*, the County Auditor shall certify in each year (beginning in the payment year 2017) the amount by which the original value has increased or decreased as a result of:

1. Change in tax exempt status of property;
2. Reduction or enlargement of the geographic boundaries of the district;
3. Change due to adjustments, negotiated or court-ordered abatements;
4. Change in the use of the property and classification;
5. Change in state law governing class rates; or
6. Change in previously issued building permits.

In any year in which the current Net Tax Capacity (NTC) value of the District declines below the ONTC, no value will be captured and no tax increment will be payable to the EDA or City.

The original local tax rate for the District will be the local tax rate for taxes payable 2016, assuming the request for certification is made before June 30, 2016. The ONTC and the Original Local Tax Rate for the District appear in the table below.

Pursuant to *M.S., Section 469.174 Subd. 4 and M.S., Section 469.177, Subd. 1, 2, and 4*, the estimated Captured Net Tax Capacity (CTC) of the District, within Redevelopment Project Area No. 1, upon completion of the projects within the District, will annually approximate tax increment revenues as shown in the table below. The EDA and City request 100 percent of the available increase in tax capacity for repayment of its obligations and current expenditures, beginning in the tax year payable 2017. The Project Tax Capacity (PTC) listed is an estimate of values when the projects within the District are completed.

<b>Project Estimated Tax Capacity upon Completion (PTC)</b>	<b>\$253,083</b>	
<b>Original Estimated Net Tax Capacity (ONTC)</b>	<b>\$7,622</b>	
<b>Estimated Captured Tax Capacity (CTC)</b>	<b>\$245,461</b>	
<b>Original Local Tax Rate</b>	<b>1.12222</b>	Estimated Pay 2015
<b>Estimated Annual Tax Increment (CTC x Local Tax Rate)</b>	<b>\$275,461</b>	
<b>Percent Retained by the EDA</b>	<b>100%</b>	

Tax capacity includes a 3% inflation factor for the duration of the District. The tax capacity included in this chart is the estimated tax capacity of the District in year 25. The tax capacity of the District in year one is estimated to be \$62,250.

Pursuant to *M.S., Section 469.177, Subd. 4*, the EDA shall, after a due and diligent search, accompany its request for certification to the County Auditor or its notice of the District enlargement pursuant to *M.S., Section 469.175, Subd. 4*, with a listing of all properties within the District or area of enlargement for which building permits have been issued during the eighteen (18) months immediately preceding approval of the TIF Plan by the municipality pursuant to *M.S., Section 469.175, Subd. 3*. The County Auditor shall increase the original net tax capacity of the District by the net tax capacity of improvements for which a building permit was issued.

**The City has reviewed the area to be included in the District and found no parcels for which building permits have been issued during the 18 months immediately preceding approval of the TIF Plan by the City.**



## **Subsection 2-9. Sources of Revenue/Bonds to be Issued**

The costs outlined in the Uses of Funds will be financed primarily through the annual collection of tax increments. The EDA or City reserves the right to incur bonds or other indebtedness as a result of the TIF Plan. As presently proposed, the projects within the District will be financed by a pay-as-you-go note/interfund loan/transfer. Any refunding amounts will be deemed a budgeted cost without a formal TIF Plan Modification. This provision does not obligate the EDA or City to incur debt. The EDA or City will issue bonds or incur other debt only upon the determination that such action is in the best interest of the City.

The total estimated tax increment revenues for the District are shown in the table below:

<b><u>SOURCES OF FUNDS</u></b>	<b><u>TOTAL</u></b>
Tax Increment	\$4,923,648
<u>Interest</u>	<u>\$492,365</u>
<b>TOTAL</b>	<b>\$5,416,013</b>

The EDA or City may issue bonds (as defined in the TIF Act) secured in whole or in part with tax increments from the District in a maximum principal amount of \$2,856,846. Such bonds may be in the form of pay-as-you-go notes, revenue bonds or notes, general obligation bonds, or interfund loans. This estimate of total bonded indebtedness is a cumulative statement of authority under this TIF Plan as of the date of approval.

## **Subsection 2-10. Uses of Funds**

Currently under consideration for the District is a proposal to facilitate construction of 166 units of affordable senior apartments. The EDA and City have determined that it will be necessary to provide assistance to the project(s) for certain District costs, as described. The EDA has studied the feasibility of the development or redevelopment of property in and around the District. To facilitate the establishment and development or redevelopment of the District, this TIF Plan authorizes the use of tax increment financing to pay for the cost of certain eligible expenses. The estimate of public costs and uses of funds associated with the District is outlined in the following table.

<b><u>USES OF TAX INCREMENT FUNDS</u></b>	<b><u>TOTAL</u></b>
Land/Building Acquisition	\$1,050,000
Site Improvements/Preparation	\$200,000
Utilities	\$300,000
Other Qualifying Improvements	\$814,482
<u>Administrative Costs (up to 10%)</u>	<u>\$492,365</u>
PROJECT COST TOTAL	\$2,856,847
<u>Interest</u>	<u>\$2,559,166</u>
<b>PROJECT AND INTEREST COSTS TOTAL</b>	<b>\$5,416,013</b>

The total project cost, including financing costs (interest) listed in the table above does not exceed the total projected tax increments for the District as shown in Subsection 2-9.

Estimated costs associated with the District are subject to change among categories without a modification to this TIF Plan. The cost of all activities to be considered for tax increment financing will not exceed, without formal modification, the budget above pursuant to the applicable statutory requirements. The EDA may expend funds for qualified housing activities outside of the District boundaries.

### **Subsection 2-11. Fiscal Disparities Election**

Pursuant to *M.S., Section 469.177, Subd. 3*, the City may elect one of two methods to calculate fiscal disparities. If the calculations pursuant to *M.S., Section 469.177, Subd. 3, clause b*, (inside the District) are followed, the following method of computation shall apply:

- (1) *The original net tax capacity shall be determined before the application of the fiscal disparity provisions of Chapter 276A or 473F. The current net tax capacity shall exclude any fiscal disparity commercial-industrial net tax capacity increase between the original year and the current year multiplied by the fiscal disparity ratio determined pursuant to M.S., Section 276A.06, subdivision 7 or M.S., Section 473F.08, subdivision 6. Where the original net tax capacity is equal to or greater than the current net tax capacity, there is no captured tax capacity and no tax increment determination. Where the original tax capacity is less than the current tax capacity, the difference between the original net tax capacity and the current net tax capacity is the captured net tax capacity. This amount less any portion thereof which the authority has designated, in its tax increment financing plan, to share with the local taxing districts is the retained captured net tax capacity of the authority.*
- (2) *The county auditor shall exclude the retained captured net tax capacity of the authority from the net tax capacity of the local taxing districts in determining local taxing district tax rates. The local tax rates so determined are to be extended against the retained captured net tax capacity of the authority as well as the net tax capacity of the local taxing districts. The tax generated by the extension of the less of (A) the local taxing district tax rates or (B) the original local tax rate to the retained captured net tax capacity of the authority is the tax increment of the authority.*

**The City will choose to calculate fiscal disparities by clause b. It is not anticipated that the District will contain commercial/industrial property. As a result, there should be no impact due to the fiscal disparities provision on the District.**

According to *M.S., Section 469.177, Subd. 3*:

- (c) The method of computation of tax increment applied to a district pursuant to paragraph (a) or (b) shall remain the same for the duration of the district, except that the governing body may elect to change its election from the method of computation in paragraph (a) to the method in paragraph (b).*

## **Subsection 2-12. Business Subsidies**

Pursuant to *M.S., Section 116J.993, Subd. 3*, the following forms of financial assistance are not considered a business subsidy:

- (1) A business subsidy of less than \$150,000;
- (2) Assistance that is generally available to all businesses or to a general class of similar businesses, such as a line of business, size, location, or similar general criteria;
- (3) Public improvements to buildings or lands owned by the state or local government that serve a public purpose and do not principally benefit a single business or defined group of businesses at the time the improvements are made;
- (4) Redevelopment property polluted by contaminants as defined in *M.S., Section 116J.552, Subd. 3*;
- (5) Assistance provided for the sole purpose of renovating old or decaying building stock or bringing it up to code and assistance provided for designated historic preservation districts, provided that the assistance is equal to or less than 50% of the total cost;
- (6) Assistance to provide job readiness and training services if the sole purpose of the assistance is to provide those services;
- (7) Assistance for housing;
- (8) Assistance for pollution control or abatement, including assistance for a tax increment financing hazardous substance subdistrict as defined under *M.S., Section 469.174, Subd. 23*;
- (9) Assistance for energy conservation;
- (10) Tax reductions resulting from conformity with federal tax law;
- (11) Workers' compensation and unemployment compensation;
- (12) Benefits derived from regulation;
- (13) Indirect benefits derived from assistance to educational institutions;
- (14) Funds from bonds allocated under chapter 474A, bonds issued to refund outstanding bonds, and bonds issued for the benefit of an organization described in section 501 (c) (3) of the Internal Revenue Code of 1986, as amended through December 31, 1999;
- (15) Assistance for a collaboration between a Minnesota higher education institution and a business;
- (16) Assistance for a tax increment financing soils condition district as defined under *M.S., Section 469.174, Subd. 19*;
- (17) Redevelopment when the recipient's investment in the purchase of the site and in site preparation is 70 percent or more of the assessor's current year's estimated market value;
- (18) General changes in tax increment financing law and other general tax law changes of a principally technical nature;
- (19) Federal assistance until the assistance has been repaid to, and reinvested by, the state or local government agency;
- (20) Funds from dock and wharf bonds issued by a seaway port authority;
- (21) Business loans and loan guarantees of \$150,000 or less;

- (22) Federal loan funds provided through the United States Department of Commerce, Economic Development Administration; and
- (23) Property tax abatements granted under *M.S., Section 469.1813* to property that is subject to valuation under Minnesota Rules, chapter 8100.

The EDA will comply with *M.S., Sections 116J.993 to 116J.995* to the extent the tax increment assistance under this TIF Plan does not fall under any of the above exemptions.

### **Subsection 2-13. County Road Costs**

Pursuant to *M.S., Section 469.175, Subd. 1a*, the county board may require the EDA or City to pay for all or part of the cost of county road improvements if the proposed development to be assisted by tax increment will, in the judgment of the county, substantially increase the use of county roads requiring construction of road improvements or other road costs and if the road improvements are not scheduled within the next five years under a capital improvement plan or within five years under another county plan.

If the county elects to use increments to improve county roads, it must notify the EDA or City within forty-five days of receipt of this TIF Plan. In the opinion of the EDA and City and consultants, the proposed development outlined in this TIF Plan will have little or no impact upon county roads, therefore the TIF Plan was not forwarded to the county 45 days prior to the public hearing. The EDA and City are aware that the county could claim that tax increment should be used for county roads, even after the public hearing.

### **Subsection 2-14. Estimated Impact on Other Taxing Jurisdictions**

The estimated impact on other taxing jurisdictions assumes that the redevelopment contemplated by the TIF Plan would occur without the creation of the District. However, the EDA or City has determined that such development or redevelopment would not occur "but for" tax increment financing and that, therefore, the fiscal impact on other taxing jurisdictions is \$0. The estimated fiscal impact of the District would be as follows if the "but for" test was not met:

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**IMPACT ON TAX BASE**

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	<b><u>2014/Pay 2015 Total Net Tax Capacity</u></b>	<b><u>Estimated Captured Tax Capacity (CTC) Upon Completion</u></b>	<b><u>Percent of CTC to Entity Total</u></b>
Anoka County	285,531,892	245,461	<b>0.0860%</b>
City of Coon Rapids	49,038,831	245,461	<b>0.5005%</b>
Anoka Hennepin ISD No. 11	148,985,813	245,461	<b>0.1648%</b>

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**IMPACT ON TAX RATES**

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	<b><u>Pay 2015 Extension Rates</u></b>	<b><u>Percent of Total</u></b>	<b><u>CTC</u></b>	<b><u>Potential Taxes</u></b>
Anoka County	0.381230	33.97%	245,461	<b>93,577</b>
City of Coon Rapids	0.447540	39.88%	245,461	<b>109,854</b>
Anoka Hennepin ISD No. 11	0.224820	20.03%	245,461	<b>55,185</b>
Other	<u>0.068630</u>	<u>6.12%</u>	<u>245,461</u>	<b>16,846</b>
<b>Total</b>	1.122220	100.00%		<b>275,461</b>

The estimates listed above display the captured tax capacity when all construction is completed. The tax rate used for calculations is the actual Pay 2015 rate. The total net capacity for the entities listed above are based on actual Pay 2015 figures. The District will be certified under the actual Pay 2016 rates, which were unavailable at the time this TIF Plan was prepared.

Pursuant to *M.S. Section 469.175 Subd. 2(b)*:

- (1) Estimate of total tax increment. It is estimated that the total amount of tax increment that will be generated over the life of the District is \$4,923,648;
- (2) Probable impact of the District on city provided services and ability to issue debt. An impact of the District on police protection is not expected. The City police department does track all calls for service by owner or renter occupancy. With any addition of new residents or businesses, police calls for service will be increased. New developments add an increase in traffic, and additional overall demands to the call load. The City does not expect that the proposed development, in and of itself, will necessitate new capital investment.

The probable impact of the District on fire protection is not expected to be significant. Typically new buildings generate few calls, if any, and are of superior construction.

The impact of the District on public infrastructure is expected to be minimal. The development is not expected to significantly impact any traffic movements in the area. The current infrastructure for sanitary sewer, storm sewer and water will be able to handle the additional volume generated from the proposed development. Based on the development plans, there are no additional costs associated with street maintenance, sweeping, plowing, lighting and sidewalks. The development in the District is expected to contribute an estimated \$412,510 in sanitary sewer (SAC) connection fees.

The probable impact of any District general obligation tax increment bonds on the ability to issue debt for general fund purposes is expected to be minimal. It is not anticipated that there will be any general obligation debt issued in relation to this project, therefore there will be no impact on the City's ability to issue future debt or on the City's debt limit.

- (3) Estimated amount of tax increment attributable to school district levies. It is estimated that the amount of tax increments over the life of the District that would be attributable to school district levies, assuming the school district's share of the total local tax rate for all taxing jurisdictions remained the same, is \$986,207.
- (4) Estimated amount of tax increment attributable to county levies. It is estimated that the amount of tax increments over the life of the District that would be attributable to county levies, assuming the county's share of the total local tax rate for all taxing jurisdictions remained the same, is \$1,672,563;
- (5) Additional information requested by the county or school district. The City is not aware of any standard questions in a county or school district written policy regarding tax increment districts and impact on county or school district services. The county or school district must request additional information pursuant to *M.S. Section 469.175 Subd. 2(b)* within 15 days after receipt of the tax increment financing plan.

No requests for additional information from the county or school district regarding the proposed development for the District have been received.

## **Subsection 2-15. Supporting Documentation**

Pursuant to *M.S. Section 469.175, Subd. 1 (a), clause 7* the TIF Plan must contain identification and description of studies and analyses used to make the findings are required in the resolution approving the District. Following is a list of reports and studies on file at the City that support the EDA and City's findings:

- City of Coon Rapids: *Port Campus Square Master Plan*, 2013
- City of Coon Rapids: *Comprehensive Housing Strategy*, October, 2007
- City of Coon Rapids: *Comprehensive Plan*, August, 2009

## **Subsection 2-16. Definition of Tax Increment Revenues**

Pursuant to *M.S., Section 469.174, Subd. 25*, tax increment revenues derived from a tax increment financing district include all of the following potential revenue sources:

1. Taxes paid by the captured net tax capacity, but excluding any excess taxes, as computed under *M.S., Section 469.177*;
2. The proceeds from the sale or lease of property, tangible or intangible, to the extent the property was purchased by the Authority with tax increments;
3. Principal and interest received on loans or other advances made by the Authority with tax increments;
4. Interest or other investment earnings on or from tax increments; and
5. Repayments or return of tax increments made to the Authority under agreements for districts for which the request for certification was made after August 1, 1993.



## **Subsection 2-17. Modifications to the District**

In accordance with *M.S., Section 469.175, Subd. 4*, any:

1. Reduction or enlargement of the geographic area of the District, if the reduction does not meet the requirements of *M.S., Section 469.175, Subd. 4(e)*;
2. Increase in amount of bonded indebtedness to be incurred;
3. A determination to capitalize interest on debt if that determination was not a part of the original TIF Plan;
4. Increase in the portion of the captured net tax capacity to be retained by the EDA or City;
5. Increase in the estimate of the cost of the District, including administrative expenses, that will be paid or financed with tax increment from the District; or
6. Designation of additional property to be acquired by the EDA or City,

shall be approved upon the notice and after the discussion, public hearing and findings required for approval of the original TIF Plan.

Pursuant to *M.S. Section 469.175 Subd. 4(f)*, the geographic area of the District may be reduced, but shall not be enlarged after five years following the date of certification of the original net tax capacity by the county auditor. If a housing district is enlarged, the reasons and supporting facts for the determination that the addition to the district meets the criteria of *M.S., Section 469.174, Subd. 11* must be documented. The requirements of this paragraph do not apply if (1) the only modification is elimination of parcel(s) from the District and (2) (A) the current net tax capacity of the parcel(s) eliminated from the District equals or exceeds the net tax capacity of those parcel(s) in the District's original net tax capacity or (B) the EDA agrees that, notwithstanding *M.S., Section 469.177, Subd. 1*, the original net tax capacity will be reduced by no more than the current net tax capacity of the parcel(s) eliminated from the District.

The EDA or City must notify the County Auditor of any modification to the District. Modifications to the District in the form of a budget modification or an expansion of the boundaries will be recorded in the TIF Plan.

## **Subsection 2-18. Administrative Expenses**

In accordance with *M.S., Section 469.174, Subd. 14*, administrative expenses means all expenditures of the EDA or City, *other than*:

1. Amounts paid for the purchase of land;
2. Amounts paid to contractors or others providing materials and services, including architectural and engineering services, directly connected with the physical development of the real property in the District;
3. Relocation benefits paid to or services provided for persons residing or businesses located in the District; or
4. Amounts used to pay principal or interest on, fund a reserve for, or sell at a discount bonds issued pursuant to *M.S., Section 469.178*; or
5. Amounts used to pay other financial obligations to the extent those obligations were used to finance costs described in clauses (1) to (3).

For districts for which the request for certification were made before August 1, 1979, or after June 30, 1982, and before August 1, 2001, administrative expenses also include amounts paid for services provided by bond counsel, fiscal consultants, and planning or economic development consultants. Pursuant to *M.S., Section*

469.176, Subd. 3, tax increment may be used to pay any **authorized and documented** administrative expenses for the District up to but not to exceed 10 percent of the total estimated tax increment expenditures authorized by the TIF Plan or the total tax increments, as defined by M.S., Section 469.174, Subd. 25, clause (1), from the District, whichever is less.

For districts for which certification was requested after July 31, 2001, no tax increment may be used to pay any administrative expenses for District costs which exceed ten percent of total estimated tax increment expenditures authorized by the TIF Plan or the total tax increments, as defined in M.S., Section 469.174, Subd. 25, clause (1), from the District, whichever is less.

Pursuant to M.S., Section 469.176, Subd. 4h, tax increments may be used to pay for the County's actual administrative expenses incurred in connection with the District and are not subject to the percentage limits of M.S., Section 469.176, Subd. 3. The county may require payment of those expenses by February 15 of the year following the year the expenses were incurred.

Pursuant to M.S., Section 469.177, Subd. 11, the County Treasurer shall deduct an amount (currently .36 percent) of any increment distributed to the EDA or City and the County Treasurer shall pay the amount deducted to the State Commissioner of Management and Budget for deposit in the state general fund to be appropriated to the State Auditor for the cost of financial reporting of tax increment financing information and the cost of examining and auditing authorities' use of tax increment financing. This amount may be adjusted annually by the Commissioner of Revenue.

## **Subsection 2-19.      Limitation of Increment**

The tax increment pledged to the payment of bonds and interest thereon may be discharged and the District may be terminated if sufficient funds have been irrevocably deposited in the debt service fund or other escrow account held in trust for all outstanding bonds to provide for the payment of the bonds at maturity or redemption date.

Pursuant to M.S., Section 469.176, Subd. 6:

*if, after four years from the date of certification of the original net tax capacity of the tax increment financing district pursuant to M.S., Section 469.177, no demolition, rehabilitation or renovation of property or other site preparation, including qualified improvement of a street adjacent to a parcel but not installation of utility service including sewer or water systems, has been commenced on a parcel located within a tax increment financing district by the authority or by the owner of the parcel in accordance with the tax increment financing plan, no additional tax increment may be taken from that parcel and the original net tax capacity of that parcel shall be excluded from the original net tax capacity of the tax increment financing district. If the authority or the owner of the parcel subsequently commences demolition, rehabilitation or renovation or other site preparation on that parcel including qualified improvement of a street adjacent to that parcel, in accordance with the tax increment financing plan, the authority shall certify to the county auditor that the activity has commenced and the county auditor shall certify the net tax capacity thereof as most recently certified by the commissioner of revenue and add it to the original net tax capacity of the tax increment financing district. The county auditor must enforce the provisions of this subdivision. The authority must submit to the county auditor evidence that the required activity has taken place for each parcel in the district. The evidence for a parcel must be submitted by February 1 of the fifth year following the year in which the parcel was certified as included in the district. For purposes of this subdivision, qualified improvements of a*

*street are limited to (1) construction or opening of a new street, (2) relocation of a street, and (3) substantial reconstruction or rebuilding of an existing street.*

The EDA or City or a property owner must improve parcels within the District by approximately April 2019 and report such actions to the County Auditor.

#### **Subsection 2-20. Use of Tax Increment**

The EDA or City hereby determines that it will use 100 percent of the captured net tax capacity of taxable property located in the District for the following purposes:

1. To pay the principal of and interest on bonds issued to finance a project;
2. To finance, or otherwise pay the cost of redevelopment of the Redevelopment Project Area No. 1 pursuant to *M.S., Sections 469.090 to 469.1082*;
3. To pay for project costs as identified in the budget set forth in the TIF Plan;
4. To finance, or otherwise pay for other purposes as provided in *M.S., Section 469.176, Subd. 4*;
5. To pay principal and interest on any loans, advances or other payments made to or on behalf of the EDA or City or for the benefit of Redevelopment Project Area No. 1 by a developer;
6. To finance or otherwise pay premiums and other costs for insurance or other security guaranteeing the payment when due of principal of and interest on bonds pursuant to the TIF Plan or pursuant to *M.S., Chapter 462C, M.S., Sections 469.152 through 469.165*, and/or *M.S., Sections 469.178*; and
7. To accumulate or maintain a reserve securing the payment when due of the principal and interest on the tax increment bonds or bonds issued pursuant to *M.S., Chapter 462C, M.S., Sections 469.152 through 469.165*, and/or *M.S., Sections 469.178*.

**Revenues derived from tax increment from a housing district must be used solely to finance the cost of housing projects as defined in *M.S., Sections 469.174, Subd. 11 and 469.1761*. The cost of public improvements directly related to the housing projects and the allocated administrative expenses of the EDA or City may be included in the cost of a housing project.**

These revenues shall not be used to circumvent any levy limitations applicable to the City nor for other purposes prohibited by *M.S., Section 469.176, Subd. 4*.

Tax increments generated in the District will be paid by Anoka County to the EDA for the Tax Increment Fund of said District. The EDA or City will pay to the developer(s) annually an amount not to exceed an amount as specified in a developer's agreement to reimburse the costs of land acquisition, public improvements, demolition and relocation, site preparation, and administration. Remaining increment funds will be used for EDA or City administration (up to 10 percent) and for the costs of public improvement activities outside the District.

#### **Subsection 2-21. Excess Increments**

Excess increments, as defined in *M.S., Section 469.176, Subd. 2*, shall be used only to do one or more of the following:

1. Prepay any outstanding bonds;
2. Discharge the pledge of tax increment for any outstanding bonds;
3. Pay into an escrow account dedicated to the payment of any outstanding bonds; or
4. Return the excess to the County Auditor for redistribution to the respective taxing jurisdictions in proportion to their local tax rates.

The EDA or City must spend or return the excess increments under paragraph (c) within nine months after the end of the year. In addition, the EDA or City may, subject to the limitations set forth herein, choose to modify the TIF Plan in order to finance additional public costs in Redevelopment Project Area No. 1 or the District.

#### **Subsection 2-22. Requirements for Agreements with the Developer**

The EDA or City will review any proposal for private development to determine its conformance with the Redevelopment Plan and with applicable municipal ordinances and codes. To facilitate this effort, the following documents may be requested for review and approval: site plan, construction, mechanical, and electrical system drawings, landscaping plan, grading and storm drainage plan, signage system plan, and any other drawings or narrative deemed necessary by the EDA or City to demonstrate the conformance of the development with City plans and ordinances. The EDA or City may also use the Agreements to address other issues related to the development.

Pursuant to *M.S., Section 469.176, Subd. 5*, no more than 10 percent, by acreage, of the property to be acquired in the District as set forth in the TIF Plan shall at any time be owned by the EDA or City as a result of acquisition with the proceeds of bonds issued pursuant to *M.S., Section 469.178* to which tax increments from property acquired is pledged, unless prior to acquisition in excess of 10 percent of the acreage, the EDA or City concluded an agreement for the development of the property acquired and which provides recourse for the EDA or City should the development not be completed.

#### **Subsection 2-23. Assessment Agreements**

Pursuant to *M.S., Section 469.177, Subd. 8*, the EDA or City may enter into a written assessment agreement in recordable form with the developer of property within the District which establishes a minimum market value of the land and completed improvements for the duration of the District. The assessment agreement shall be presented to the County Assessor who shall review the plans and specifications for the improvements to be constructed, review the market value previously assigned to the land upon which the improvements are to be constructed and, so long as the minimum market value contained in the assessment agreement appears, in the judgment of the assessor, to be a reasonable estimate, the County Assessor shall also certify the minimum market value agreement.

#### **Subsection 2-24. Administration of the District**

Administration of the District will be handled by the Community Development Specialist.

#### **Subsection 2-25. Annual Disclosure Requirements**

Pursuant to *M.S., Section 469.175, Subds. 5, 6, and 6b* the EDA or City must undertake financial reporting for all tax increment financing districts to the Office of the State Auditor, County Board and County Auditor on or before August 1 of each year. *M.S., Section 469.175, Subd. 5* also provides that an annual statement shall be published in a newspaper of general circulation in the City on or before August 15.

If the City fails to make a disclosure or submit a report containing the information required by *M.S., Section 469.175 Subd. 5 and Subd. 6*, the OSA will direct the County Auditor to withhold the distribution of tax increment from the District.

## **Subsection 2-26. Reasonable Expectations**

As required by the TIF Act, in establishing the District, the determination has been made that the anticipated development would not reasonably be expected to occur solely through private investment within the reasonably foreseeable future. In making said determination, reliance has been placed upon written representation made by the developer to such effects and upon EDA and City staff awareness of the feasibility of developing the project site(s) within the District.

## **Subsection 2-27. Other Limitations on the Use of Tax Increment**

1. General Limitations. All revenue derived from tax increment shall be used in accordance with the TIF Plan. The revenues shall be used to finance, or otherwise pay the cost of redevelopment of the Redevelopment Project Area No. 1 pursuant to *M.S., Sections 469.090 to 469.1082*. Tax increments may not be used to circumvent existing levy limit law. No tax increment may be used for the acquisition, construction, renovation, operation, or maintenance of a building to be used primarily and regularly for conducting the business of a municipality, county, school district, or any other local unit of government or the state or federal government. This provision does not prohibit the use of revenues derived from tax increments for the construction or renovation of a parking structure.
2. Housing District Exceptions to Restriction on Pooling; Five Year Limit. Pursuant to *M.S., Section 469.1763*, (1) At least 80% of the tax increment derived from the District must be expended on Public Costs incurred within said district, and up to 20% of said tax increments may be spent on Public Costs incurred outside of the District but within Redevelopment Project Area No. 1; provided that in the case of a housing district, a housing project, as defined in *M.S., Section 469.174, Subd. 11*, is deemed to be an activity in the District, even if the expenditure occurred after five years.

## **Subsection 2-28. Summary**

The Coon Rapids is establishing the District to provide an impetus for residential development and provide safe and decent life cycle housing in the City. The TIF Plan for the District was prepared by Ehlers & Associates, Inc., 3060 Centre Pointe Drive, Roseville, Minnesota 55113-1105, telephone (651) 697-8500.

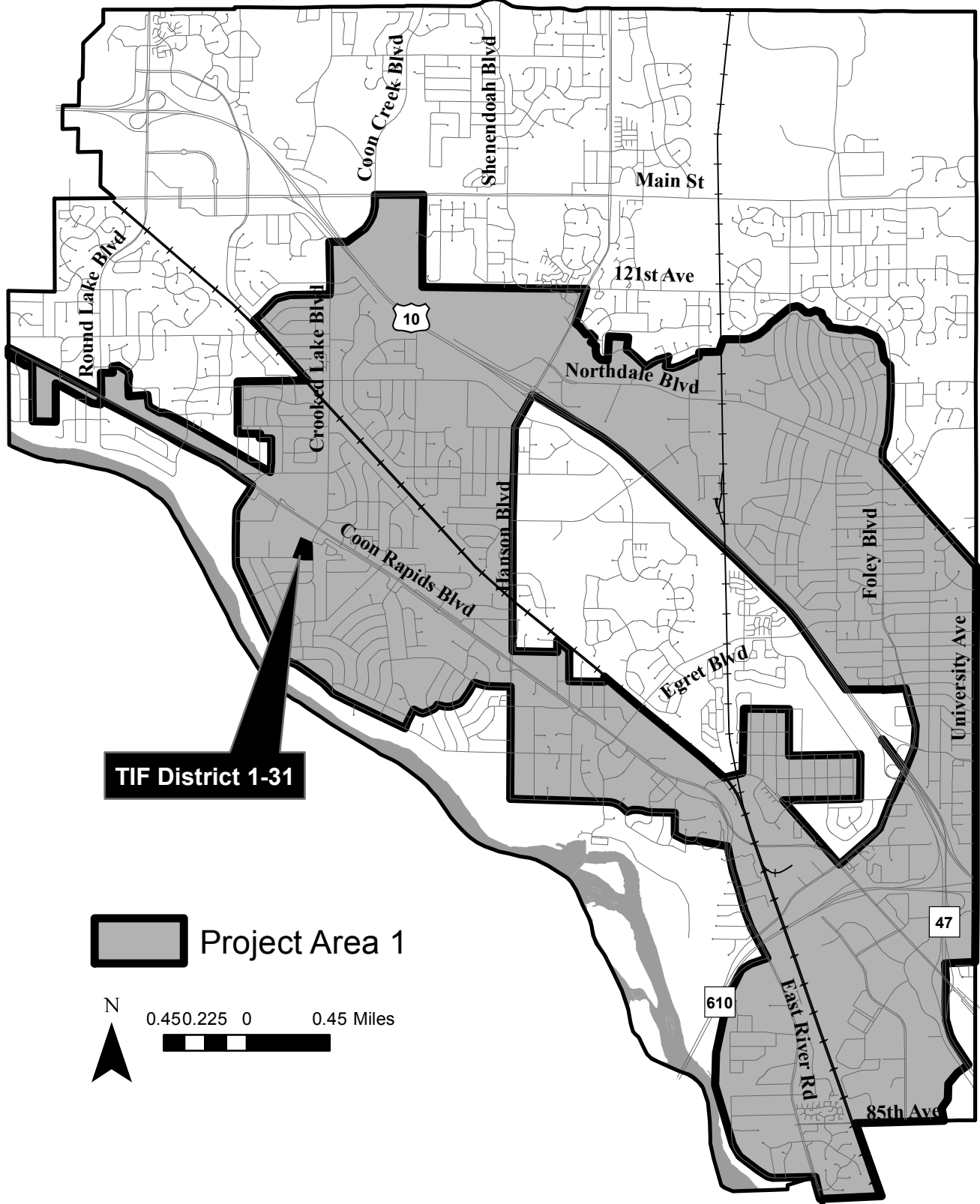
## Appendix A

The proposed project consists of a 4-story, 166-unit rental building situated on approximately 4.5 acres in the city. The building will be oriented toward individuals age 55 and up and at least 40% of the units will be affordable to households at or below 60% of the area median income. The project will contain a mix of one and two bedroom units with both surface and underground parking for residents.



## Appendix B

### Map of Redevelopment Project Area No. 1 and the District



## Appendix C

### Description of Property to be Included in the District

The District encompasses all property and adjacent rights-of-way and abutting roadways identified by the parcels listed below.

<u>Parcel Numbers</u>	<u>Address</u>	<u>Owner</u>
16-31-24-34-0021	10920 Crooked Lake Blvd NW	Coon Rapids HRA
16-31-24-34-0022	10930 Crooked Lake Blvd NW	Szyplinski
16-31-24-34-0031*	11000 Crooked Lake Blvd NW	Coon Rapids EDA

\* Project will only include 4.72 acres of the parcel.

## Appendix D

### Estimated Cash Flow for the District



## Dominium

City of Coon Rapids

166 unit 55+ Affordable (100%) Senior Apartment

### ASSUMPTIONS AND RATES

DistrictType: Housing

District Name/Number:

County District #:

First Year Construction or Inflation on Value

2015

Existing District - Specify No. Years Remaining

Inflation Rate - Every Year:

3.00%

Interest Rate:

6.00%

Present Value Date:

1-Feb-16

First Period Ending

1-Aug-16

Tax Year District was Certified:

Pay 2016

Cashflow Assumes First Tax Increment For Development:

2017

Years of Tax Increment

26

Assumes Last Year of Tax Increment

2042

Fiscal Disparities Election [Outside (A), Inside (B), or NA]

Inside(B)

Incremental or Total Fiscal Disparities

Incremental

Fiscal Disparities Contribution Ratio

35.5354% Pay 2015

Fiscal Disparities Metro-Wide Tax Rate

161.6250% Pay 2015

Maximum/Frozen Local Tax Rate:

112.222% Pay 2015

Current Local Tax Rate: (Use lesser of Current or Max.)

112.222% Pay 2015

State-wide Tax Rate (Comm./Ind. only used for total taxes)

50.8400% Pay 2015

Market Value Tax Rate (Used for total taxes)

0.21266% Pay 2015

#### Tax Rates

Exempt Class Rate (Exempt)	0.00%
Commercial Industrial Preferred Class Rate (C/I Pref.)	
First \$150,000	1.50%
Over \$150,000	2.00%
Commercial Industrial Class Rate (C/I)	2.00%
Rental Housing Class Rate (Rental)	1.25%
Affordable Rental Housing Class Rate (Aff. Rental)	
First \$100,000	0.75%
Over \$100,000	0.25%
Non-Homestead Residential (Non-H Res. 1 Unit)	
First \$500,000	1.00%
Over \$500,000	1.25%
Homestead Residential Class Rate (Hmstd. Res.)	
First \$500,000	1.00%
Over \$500,000	1.25%
Agricultural Non-Homestead	1.00%

### BASE VALUE INFORMATION (Original Tax Capacity)

Map #	PID	Owner	Address	Land Market Value	Building Market Value	Total Market Value	Percentage Of Value Used for District	Original Market Value	Tax Year Original Market Value	Property Tax Class	Current Original Tax Capacity	Class After Conversion	After Conversion Orig. Tax Cap.	Area/ Phase
	16-31-24-34-0031	EDA		796,701	0	796,701	100%	796,701	Pay 2016	Exempt	-	Aff. Rental	5,975	
	16-31-24-34-0022	Szyplinski		46,000	127,500	173,500	100%	173,500	Pay 2016	Hmstd. Res.	1,735	Aff. Rental	1,301	
	16-31-24-34-0021	HRA		46,000	0	46,000	100%	46,000	Pay 2016	Exempt	-	Aff. Rental	345	
				888,701	127,500	1,016,201		1,016,201			1,735		7,622	

#### Note:

- Base values for parcels 0031 and 0021 are for pay 2016 based on a review of the county website on 3-6-15. The project will use 4.72 acres (205,866 sq. ft.) of parcel 16-31-24-34-0031. The value of parcel 16-31-24-34-0031 will be \$3.87 per sq. ft. (\$796,701) based on information provided by the city assessor.
- Properties will be combined through a platting process for the project



### Dominium

City of Coon Rapids

166 unit 55+ Affordable (100%) Senior Apartment

PROJECT INFORMATION (Project Tax Capacity)												
Area/Phase	New Use	Estimated Market Value Per Sq. Ft./Unit	Taxable Market Value Per Sq. Ft./Unit	Total Sq. Ft./Units	Total Taxable Market Value	Property Tax Class	Project Tax Capacity	Percentage Completed 2015	Percentage Completed 2016	Percentage Completed 2017	Percentage Completed 2018	First Year Full Taxes Payable
	Residential	100,000	100,000	166	16,600,000	Aff. Rental	124,500	50%	100%	100%	100%	2018
<b>TOTAL</b>					<b>16,600,000</b>		<b>124,500</b>					

**Note:**

1. Market values are based upon estimates from city staff after discussions with the city assessor

TAX CALCULATIONS									
New Use	Total Tax Capacity	Fiscal Disparities Tax Capacity	Local Tax Capacity	Local Property Taxes	Fiscal Disparities Taxes	State-wide Property Taxes	Market Value Taxes	Total Taxes	Taxes Per Sq. Ft./Unit
Residential	124,500	0	124,500	139,716	0	0	35,302	175,018	1,054.33
<b>TOTAL</b>	<b>124,500</b>	<b>0</b>	<b>124,500</b>	<b>139,716</b>	<b>0</b>	<b>0</b>	<b>35,302</b>	<b>175,018</b>	

**Note:**

1. Taxes and tax increment will vary significantly from year to year depending upon values, rates, state law, fiscal disparities and other factors which cannot be predicted.
2. Assumes certification in 2016. Tax rates may be lower for 2016 which would result in less tax increment.

WHAT IS EXCLUDED FROM TIF?	
Total Property Taxes	175,018
less State-wide Taxes	0
less Fiscal Disp. Adj.	0
less Market Value Taxes	(35,302)
less Base Value Taxes	(8,553)
<b>Annual Gross TIF</b>	<b>131,163</b>





**Dominium**  
**City of Coon Rapids**  
**166 unit 55+ Affordable (100%) Senior Apartment**

TAX INCREMENT CASH FLOW														
% of OTC	Project Tax Capacity	Original Tax Capacity	Fiscal Disparities Incremental	Captured Tax Capacity	Local Tax Rate	Annual Gross Tax Increment	Semi-Annual Gross Tax Increment	State Auditor 0.36%	Admin. at 10%	Semi-Annual Net Tax Increment	Semi-Annual Present Value	PERIOD ENDING Yrs.	Tax Year	Payment Date
							-	-	-	-				08/01/16
														02/01/17
100%	62,250	(7,622)	-	54,628	112.222%	61,305	30,653	(110)	(3,054)	27,488	25,155	0.5	2017	08/01/17
							30,653	(110)	(3,054)	27,488	49,578	1	2017	02/01/18
100%	124,500	(7,622)	-	116,878	112.222%	131,163	65,582	(236)	(6,535)	58,811	100,309	1.5	2018	08/01/18
							65,582	(236)	(6,535)	58,811	149,562	2	2018	02/01/19
100%	128,235	(7,622)	-	120,613	112.222%	135,355	67,677	(244)	(6,743)	60,690	198,909	2.5	2019	08/01/19
							67,677	(244)	(6,743)	60,690	246,819	3	2019	02/01/20
100%	132,082	(7,622)	-	124,461	112.222%	139,672	69,836	(251)	(6,958)	62,626	294,817	3.5	2020	08/01/20
							69,836	(251)	(6,958)	62,626	341,416	4	2020	02/01/21
100%	136,045	(7,622)	-	128,423	112.222%	144,119	72,059	(259)	(7,180)	64,620	388,099	4.5	2021	08/01/21
							72,059	(259)	(7,180)	64,620	433,422	5	2021	02/01/22
100%	140,126	(7,622)	-	132,504	112.222%	148,699	74,350	(268)	(7,408)	66,674	478,824	5.5	2022	08/01/22
							74,350	(268)	(7,408)	66,674	522,903	6	2022	02/01/23
100%	144,330	(7,622)	-	136,708	112.222%	153,417	76,708	(276)	(7,643)	68,789	567,056	6.5	2023	08/01/23
							76,708	(276)	(7,643)	68,789	609,923	7	2023	02/01/24
100%	148,660	(7,622)	-	141,038	112.222%	158,276	79,138	(285)	(7,885)	70,968	652,860	7.5	2024	08/01/24
							79,138	(285)	(7,885)	70,968	694,546	8	2024	02/01/25
100%	153,119	(7,622)	-	145,498	112.222%	163,281	81,640	(294)	(8,135)	73,212	736,297	8.5	2025	08/01/25
							81,640	(294)	(8,135)	73,212	776,833	9	2025	02/01/26
100%	157,713	(7,622)	-	150,091	112.222%	168,436	84,218	(303)	(8,391)	75,523	817,430	9.5	2026	08/01/26
							84,218	(303)	(8,391)	75,523	856,845	10	2026	02/01/27
100%	162,444	(7,622)	-	154,823	112.222%	173,745	86,873	(313)	(8,656)	77,904	896,318	10.5	2027	08/01/27
							86,873	(313)	(8,656)	77,904	934,642	11	2027	02/01/28
100%	167,318	(7,622)	-	159,696	112.222%	179,214	89,607	(323)	(8,928)	80,356	973,020	11.5	2028	08/01/28
							89,607	(323)	(8,928)	80,356	1,010,281	12	2028	02/01/29
100%	172,337	(7,622)	-	164,716	112.222%	184,847	92,424	(333)	(9,209)	82,882	1,047,594	12.5	2029	08/01/29
							92,424	(333)	(9,209)	82,882	1,083,819	13	2029	02/01/30
100%	177,507	(7,622)	-	169,886	112.222%	190,649	95,325	(343)	(9,498)	85,483	1,120,094	13.5	2030	08/01/30
							95,325	(343)	(9,498)	85,483	1,155,312	14	2030	02/01/31
100%	182,832	(7,622)	-	175,211	112.222%	196,625	98,313	(354)	(9,796)	88,163	1,190,576	14.5	2031	08/01/31
							98,313	(354)	(9,796)	88,163	1,224,813	15	2031	02/01/32
100%	188,317	(7,622)	-	180,696	112.222%	202,781	101,390	(365)	(10,103)	90,923	1,259,093	15.5	2032	08/01/32
							101,390	(365)	(10,103)	90,923	1,292,375	16	2032	02/01/33
100%	193,967	(7,622)	-	186,345	112.222%	209,121	104,560	(376)	(10,418)	93,765	1,325,697	16.5	2033	08/01/33
							104,560	(376)	(10,418)	93,765	1,358,050	17	2033	02/01/34
100%	199,786	(7,622)	-	192,164	112.222%	215,651	107,825	(388)	(10,744)	96,693	1,390,440	17.5	2034	08/01/34
							107,825	(388)	(10,744)	96,693	1,421,888	18	2034	02/01/35
100%	205,780	(7,622)	-	198,158	112.222%	222,377	111,188	(400)	(11,079)	99,709	1,453,371	18.5	2035	08/01/35
							111,188	(400)	(11,079)	99,709	1,483,938	19	2035	02/01/36
100%	211,953	(7,622)	-	204,331	112.222%	229,305	114,652	(413)	(11,424)	102,816	1,514,539	19.5	2036	08/01/36
							114,652	(413)	(11,424)	102,816	1,544,248	20	2036	02/01/37
100%	218,312	(7,622)	-	210,690	112.222%	236,441	118,220	(426)	(11,779)	106,015	1,573,990	20.5	2037	08/01/37
							118,220	(426)	(11,779)	106,015	1,602,865	21	2037	02/01/38
100%	224,861	(7,622)	-	217,239	112.222%	243,790	121,895	(439)	(12,146)	109,311	1,631,771	21.5	2038	08/01/38
							121,895	(439)	(12,146)	109,311	1,659,835	22	2038	02/01/39
100%	231,607	(7,622)	-	223,985	112.222%	251,361	125,680	(452)	(12,523)	112,705	1,687,928	22.5	2039	08/01/39
							125,680	(452)	(12,523)	112,705	1,715,203	23	2039	02/01/40
100%	238,555	(7,622)	-	230,933	112.222%	259,158	129,579	(466)	(12,911)	116,201	1,742,504	23.5	2040	08/01/40
							129,579	(466)	(12,911)	116,201	1,769,011	24	2040	02/01/41
100%	245,712	(7,622)	-	238,090	112.222%	267,189	133,595	(481)	(13,311)	119,802	1,795,542	24.5	2041	08/01/41
							133,595	(481)	(13,311)	119,802	1,821,301	25	2041	02/01/42
100%	253,083	(7,622)	-	245,461	112.222%	275,462	137,731	(496)	(13,723)	123,511	1,847,084	25.5	2042	08/01/42
							137,731	(496)	(13,723)	123,511	1,872,117	26	2042	02/01/43
<b>Total</b>							<b>4,941,437</b>	<b>(17,789)</b>	<b>(492,365)</b>	<b>4,431,283</b>				
<b>Present Value From 02/01/2016</b>							<b>2,087,645</b>	<b>(7,516)</b>	<b>(208,013)</b>	<b>1,872,117</b>				

## Appendix E

### Housing Qualifications for the District

<b>INCOME RESTRICTIONS - ADJUSTED FOR FAMILY SIZE (HOUSING DISTRICT) - ANOKA COUNTY METRO AREA MEDIAN INCOME: \$82,900</b>		
No. of Persons	50% of Median Income	60% of Median Income
1-person	\$29,400	\$35,280
2-person	\$33,600	\$40,320
3-person	\$37,800	\$45,360
4-person	\$42,000	\$50,400

Source: Department of Housing and Urban Development and Minnesota Housing Finance Agency

The two options for income limits on a standard housing district are 20% of the units at 50% of median income or 40% of the units at 60% of median income. There are no rent restrictions for a housing district.

\*\*\*PLEASE NOTE: THESE NUMBERS ARE ADJUSTED ANNUALLY. ALL INCOME FIGURES REPORTED ON THIS PAGE ARE FOR 2014. UPDATED NUMBERS FOR THE YEAR 2015 ARE NOT YET AVAILABLE.

## Appendix F

### Findings for the District

The reasons and facts supporting the findings for the adoption of the Tax Increment Financing Plan for Tax Increment Financing District No. 1-31, as required pursuant to Minnesota Statutes, Section 469.175, Subdivision 3 are as follows:

1. *Finding that Tax Increment Financing District No. 1-31 is a housing district as defined in M.S., Section 469.174, Subd. 11.*

TIF District No. 1-31 consists of 3 parcels. The development will consist of senior rental apartment housing. All or a portion of which will receive tax increment assistance and will meet income restrictions described in *M.S. 469.1761*. At least 40 percent of the units/homes receiving assistance will have incomes at or below 60 percent of area median income. Appendix E of the TIF Plan contains background for the above finding.

2. *Finding that the proposed development, in the opinion of the City Council, would not reasonably be expected to occur solely through private investment within the reasonably foreseeable future.*

*The proposed development, in the opinion of the City, would not reasonably be expected to occur solely through private investment within the reasonably foreseeable future:* This finding is supported by the fact that the development proposed in this plan provides for housing that meets the City's objectives for development and redevelopment. Due to the high cost of building affordable new housing in the City and the cost of financing the proposed public improvements, this project is feasible only through assistance, in part, from tax increment financing. The developer was asked for and provided an application and a proforma as justification that the developer would not have gone forward without tax increment assistance.

*The increased market value of the site that could reasonably be expected to occur without the use of tax increment financing would be less than the increase in market value estimated to result from the proposed development after subtracting the present value of the projected tax increments for the maximum duration of the TIF District permitted by the TIF Plan:* The City reasonably determines that no other development of similar scope is anticipated on this site without substantially similar assistance being provided to the development.

3. *Finding that the TIF Plan for Tax Increment Financing District No. 1-31 conforms to the general plan for the development or redevelopment of the municipality as a whole.*

The Planning Commission reviewed the TIF Plan and found that the TIF Plan conforms to the general development plan of the City.

4. *Finding that the TIF Plan for Tax Increment Financing District No. 1-31 will afford maximum opportunity, consistent with the sound needs of the City as a whole, for the development or redevelopment of Redevelopment Project Area No. 1 by private enterprise.*

Through the implementation of the TIF Plan, the EDA or City will provide an impetus for residential development, which is desirable or necessary for increased population and an increased need for life-cycle housing within the City.



## City Council Regular

22.

**Meeting Date:** 04/07/2015

**Subject:** Consider Foley Boulevard Station Area Plan

**From:** Matt Brown, Economic Development  
Coordinator

---

### **INTRODUCTION**

The Council is asked to consider a revised version of the Foley Boulevard Station Area Plan. While the Commission recommended approval of a previous version of the Station Area Plan in November 2014, the City Council expressed concerns about the policies recommended by that Plan. Staff and the consultant have since modified the document.

### **DISCUSSION**

**Background.** Funded by a grant from the Metropolitan Council, this planning initiative is intended to guide future development and infrastructure improvements near the Foley Boulevard Park-and-Ride facility to support and accommodate future transportation investments. The City's Comprehensive Plan, adopted in 2008, identified this area as a potential growth area and recommended further study of land uses.

**Process.** An initial draft of the Foley Boulevard Station Area Plan was completed in the fall of 2014. The initial plan included a "mixed use" scenario, which recommended commercial, office, and industrial development along Foley Boulevard and high-density residential development north of Foley Boulevard. The Council considered this plan in November 2014, but postponed consideration and requested another work session on the topic. At its work session on December 9, 2014, the Council indicated that it no longer supported the mixed use scenario, based in part on concerns from nearby property owners and skepticism of the viability of future residential development at that location. The City Council met in another work session on February 10 to provide additional feedback on the Station Area Plan. Since that time, Staff has worked with the consultant to devise a new plan.

**Revised Plan.** The new plan envisions continued industrial development, but allows for future redevelopment with commercial, office, and industrial uses. The revised plan eliminates the "business park" concept discussed at the February 10 work session. Rather than focusing on transition to different land uses, the revised plan recommends various infrastructure improvements that will support new development and capitalize on existing and future transit service. Staff is not recommending formal adoption of the Plan into the City's Comprehensive Plan. The plan's key recommendations include:

1. Continued light industrial uses with accommodation of other employment-based uses, including flex/showroom, commercial, and office, in the long term.
2. New development could include higher lot coverage, multi-story buildings that could provide higher job density than traditional industrial uses.
3. New local streets north of Foley Boulevard could facilitate redevelopment of plastics site, create new development sites, and improve access to existing properties. Some of these streets could be constructed in conjunction with Anoka County's overpass project or construction of a rail platform for Northern Lights Express.
4. Improved access to Highway 610 may be necessary to attract higher intensity employment users.
5. Coordinate with Anoka County on Foley Boulevard reconstruction and MnDOT on continued planning for Northern Lights Express.

6. Construct sidewalks and trails to improve access to the transit station.

**RECOMMENDATION**

Adopt the *Foley Boulevard Station Area Plan*.

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**Attachments**

Foley Boulevard Station Area Plan

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# ***FOLEY BOULEVARD***

## ***STATION AREA PLAN***

## ACKNOWLEDGEMENT

*The Foley Boulevard Station Area Plan was made possible in part through a [Livable Communities Demonstration Account \(LCDA\)](#) grant furnished by the Metropolitan Council. The LCDA program provides grants to support innovative redevelopment projects that link housing, jobs, services, and transit in an effort to create inspiring and lasting Livable Communities.*





# FOLEY BOULEVARD

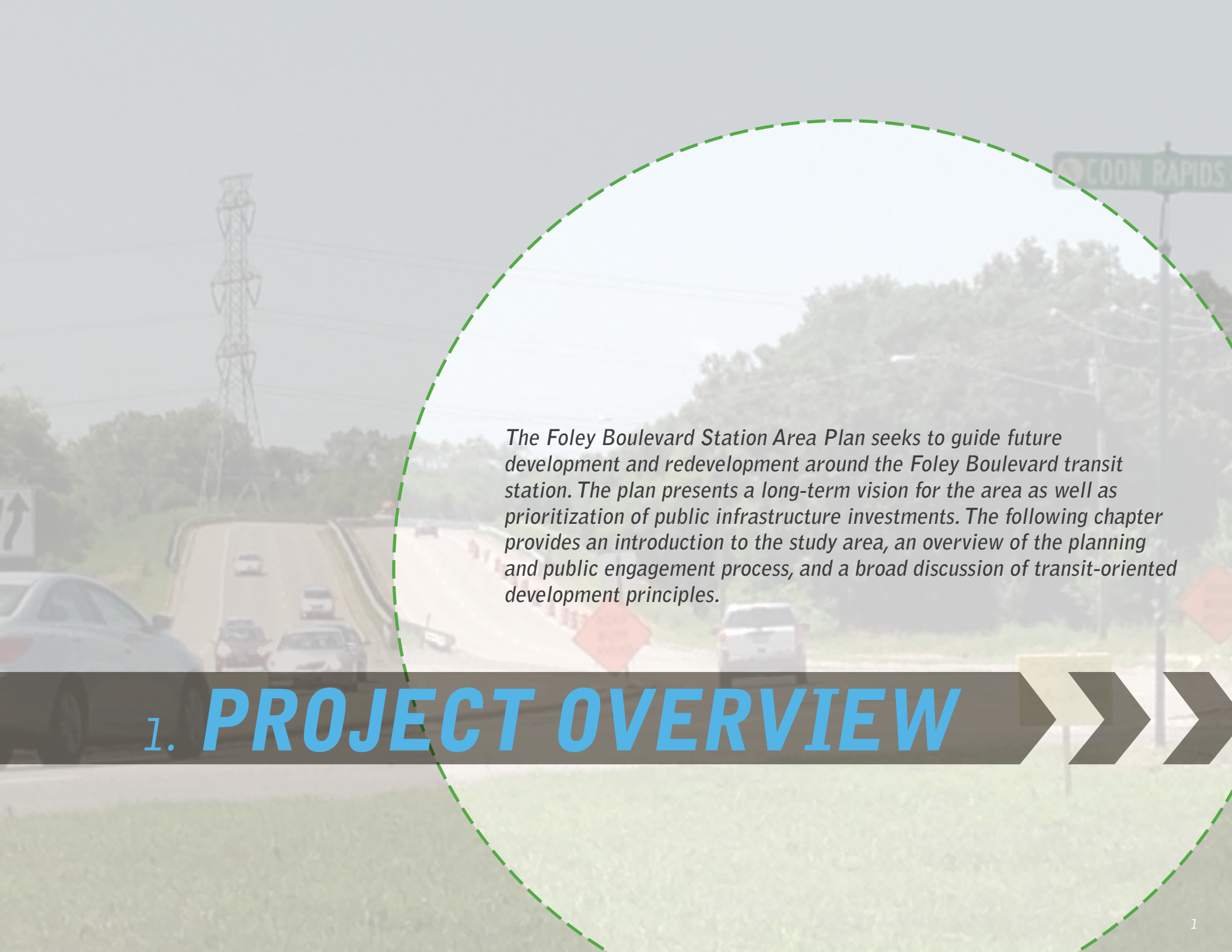
## STATION AREA PLAN

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*The Foley Boulevard Station Area Plan seeks to guide future development and redevelopment around the Foley Boulevard transit station. The plan presents a long-term vision for the area as well as prioritization of public infrastructure investments. The following chapter provides an introduction to the study area, an overview of the planning and public engagement process, and a broad discussion of transit-oriented development principles.*

# **1. PROJECT OVERVIEW**

# 1. Project Overview

## INTRODUCTION



*Foley Boulevard Park and Ride*



*Existing light industrial development*

The City of Coon Rapids developed the Foley Boulevard Station Area Plan to guide development, redevelopment, and infrastructure investment in the Foley Boulevard Station Area. The plan considers the potential for development and redevelopment as it relates to the existing transit facilities on Foley Boulevard (the Metro Transit Park and Ride facility and express bus service) as well as potential future transportation improvements such as a station for the Northern Lights Express - the proposed high speed rail to Duluth - and/or Northstar Commuter Rail.

The Foley Boulevard Station Area Plan was made possible in part through a Livable Communities Demonstration Account (LCDA) grant furnished by the Metropolitan Council to evaluate and encourage transit-oriented development (TOD). The Metropolitan Council supports opportunities for TOD around various types of transit facilities, including light rail transit (LRT), bus rapid transit (BRT), commuter rail, and high-frequency express bus service.

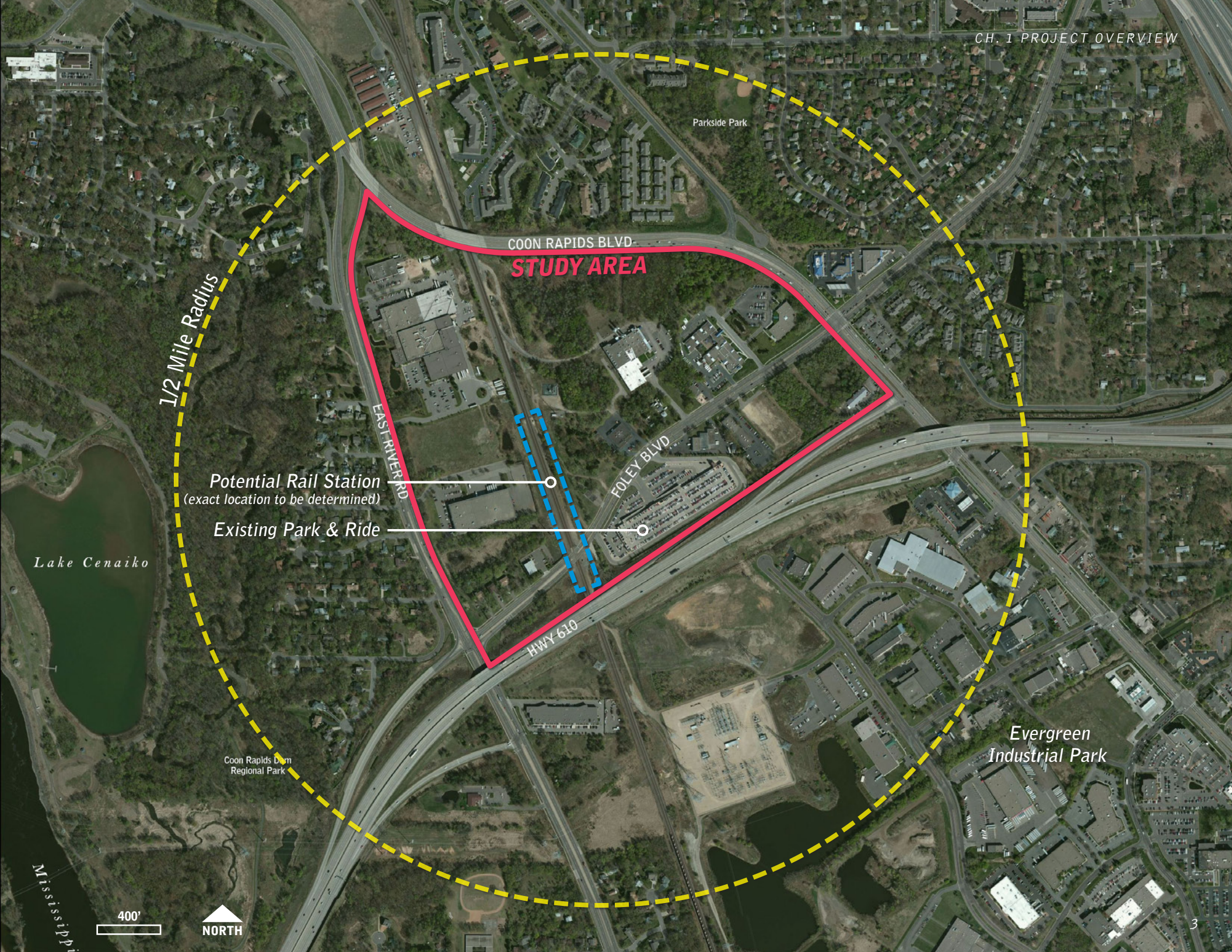
This planning effort seeks to build consensus among various agencies (MnDOT, Anoka County, Metro Transit, etc.) regarding future investments in the area, and provides long-term guidance for infrastructure improvements, transportation investment, and pedestrian/bicycle connectivity.

In the context of this plan, the Foley Boulevard “station area” is roughly defined as the area within a half-mile radius - or 10-minute walking distance from the transit station (i.e. the existing Foley Boulevard Park and Ride facility). Today, the station area includes predominantly light industrial uses located in the core area around the station. Farther out, residential neighborhoods fill the edges of the station area to the north, east, and west, along with industrial and office uses in the Evergreen Industrial Park to the south. The area also features local and regional parks, including Coon Rapids Dam Regional Park located along the Mississippi River to the west/northwest.

The plan also focuses on a more specific “study area” (shown in red in Figure 1.1 on the opposite page), which covers the area immediately adjacent to the transit station and bounded by Highway 610, Coon Rapids Boulevard, and East River Road. These two scales of analysis allow for discussion of broader connections to the station from surrounding residential and employment areas, as well as a detailed examination of redevelopment opportunities in the immediate vicinity of the station.

Figure 1.1 - Aerial View of Foley Boulevard Station Area >





Parkside Park

COON RAPIDS BLVD

**STUDY AREA**

1/2 Mile Radius

Potential Rail Station  
(exact location to be determined)

Existing Park & Ride

Lake Cenaiko

EAST RIVER RD

FOLEY BLVD

HWY 610

Coon Rapids Dam  
Regional Park

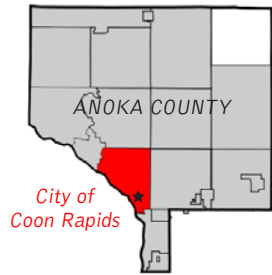
Evergreen  
Industrial Park

400'





## REGIONAL CONTEXT



The station area is located in the southeastern portion of Coon Rapids, approximately 17 miles north of downtown Minneapolis and less than a mile east of the Mississippi River. Figure 1.2 shows the regional context surrounding the station area.

### Transit

Existing express bus service connects the Foley Boulevard station to downtown Minneapolis. Ride time to downtown from Foley is approximately 25-30 minutes. The Foley Boulevard Park and Ride is well-utilized and is one of the largest in the system.

The Foley Boulevard Station area is located along the existing Northstar Commuter Rail line which connects from downtown Minneapolis to Big Lake along BNSF-owned right-of-way. Northstar does not stop at Foley today, and there are no current plans to alter Northstar station locations. The Riverdale Northstar Station is located about 7 miles northwest from the Foley Boulevard station area while the Fridley Northstar Station is about 5 miles southeast.

The proposed Northern Lights Express (NLX) route to Duluth heads north out of the station area following the BNSF freight rail corridor. NLX is a passenger rail project currently managed by MnDOT that will connect from Target Field Station in Minneapolis to the City of Duluth. The proposed route includes a north metro station at Foley Boulevard.

### Retail

Major retail centers in the regional context include Northtown Mall and Riverdale Village Shopping Center. Northtown Mall is an enclosed regional shopping center with over 600,000 square feet of retail and a variety of big box retailers in the surrounding area. Riverdale Village includes over 1.1 million square feet of retail space, including most major suburban chains. In the broader area around Riverdale Village, there is an additional 2.5 million square feet of retail.



*Fridley Northstar Station*



*Riverdale Village Shopping Center*

### Employment

Evergreen Industrial Park, a major employment center in Coon Rapids, is located just south of the station area. The Industrial Park includes over 2.5 million square feet of industrial, warehouse, and office space, and over 3,000 employees. Major tenants in the area include Bayer, Honeywell, and RMS Company.

### Other Redevelopment Areas

The southeast portion of the station area falls within the Port Evergreen district - one of four redevelopment areas along Coon Rapids Boulevard targeted by the City of Coon Rapids for office, commercial, and higher density residential development. A portion of Evergreen Industrial Park is part of the Port Evergreen district.

Located just northwest of the station area, Port Riverwalk includes areas along Coon Rapids Boulevard between Egret Boulevard and the East River Road split. The City has acquired over 30 acres of land, demolished buildings, and cleaned up contamination in this area in preparation for redevelopment. The master plan for Port Riverwalk envisions moderate to high-density housing as well as some mixed use development.



*Land use plan from the Port Riverwalk Master Plan*

Figure 1.2 - Regional Context





## TOD POTENTIAL

## WHAT IS TRANSIT ORIENTED DEVELOPMENT (TOD)?

Transit-oriented development (or TOD) aims to maximize access to public transit by encouraging walkable, moderate-to-high-density development within a half mile (or 10-minute walk) of a transit station. In addition to encouraging compact development, TOD often encourages features like a mix of housing, retail, or employment uses; parks and open space; bike and pedestrian amenities; and high quality streetscape design. Effective TOD has a number of potential benefits:

- Produces vibrant, mixed-use environments
- Encourages transit usage
- Reduces dependence on the private automobile
- Supports healthy/active lifestyles
- Expands housing and mobility choices
- Generates public and private sector revenues
- Creates places of lasting value.

## TOD POTENTIAL AT FOLEY STATION

The study found that TOD opportunities are limited in the Foley Station area. Metro Transit currently has no plans to add a Northstar stop at Foley. In addition, while the Northern Lights Express would offer a new transportation option, it is likely that most users would need to arrive at the station using automobiles given limited transit options. Thus, future TOD will be primarily related to the express bus service offered at the Park and Ride station.

## CHALLENGES AND OPPORTUNITIES

Today, the land use and development patterns in the Foley Boulevard station area are not transit-supportive. With large block sizes, low-density industrial land-uses, a limited street network, and incomplete bike and pedestrian connections, the station area presents a number of challenges to TOD.

Key considerations for facilitating TOD include:

- Encourage retail and commercial services near the station that would cater to the needs of transit riders
- Design public realm and infrastructure to enhance multi-modal access and provide desirable amenities on the site:
  - ▷ Introduce a walkable, pedestrian-friendly street network with high-quality streetscape design
  - ▷ Fill gaps in the bicycle network and provide amenities for cyclists
  - ▷ Incorporate plazas, open space, and other gathering spaces
- Create non-motorized connections to nearby neighborhoods and recreational amenities, including the Mississippi River and area parks.



*Transit-oriented development encourages walkable, mixed-use development around transit stations to encourage transit ridership and create vibrant places to live and work*



*The existing low-density land use and development patterns in the station area are not conducive to Transit-Oriented Development*

## PLANNING PROCESS



*Community stakeholders were invited to participate in an open house event to review and provide feedback on initial redevelopment concepts for the Foley Boulevard station area plan*

## ENGAGING STAKEHOLDERS

## Community Stakeholders

Community stakeholders were instrumental throughout this planning process, helping to identify key issues and opportunities and define the vision for future redevelopment in the station area. City staff engaged local businesses, property owners, and residents from the beginning of the planning process and provided a range of opportunities for input and comment through small group discussion and individual interviews, as well as two community open house events.

## Project Management Team

The Project Management Team (PMT) was comprised of City staff, key members of the consultant team, and agency representatives from Anoka County, MnDOT, Metropolitan Council, and Metro Transit. The PMT served as an advisory body for the development of the plan, helping to refine the work plan and engagement strategy, and provide input and direction around the plan content and implementation strategies.

## City Council &amp; Planning Commission

Project updates were provided for the City of Coon Rapids City Council and Planning Commission through regularly scheduled council and commission meetings as well as special work sessions. These meetings were open to the public and provided opportunities for City officials to stay apprised of the planning process and provide input and feedback on plan components.

## DEVELOPING THE PLAN

The Foley Boulevard Station Area Plan was developed through a process led by City Staff and consultants from Hoisington Koeigler Group and SEH. The process involved stakeholder engagement, data collection and analysis, exploration of alternative design concepts, and refinement of a preferred master plan concept and implementation strategies.

## Inventory and Analysis

The consultant team developed an understanding of the Foley Boulevard Station Area and key issues and opportunities through a review of existing studies and planning efforts, background data collection and analysis, and stakeholder engagement. [Chapter 2. Existing Conditions](#) provides a summary of this existing conditions analysis.

## Master Plan Development

Building on the existing conditions analysis, stakeholder input, and initial assessment of redevelopment potential in the station area, the consultant team and City staff developed a number of initial redevelopment concepts through a day-long design workshop and community open house. These redevelopment concepts addressed potential land use, roadway, and bike/pedestrian improvements that may be appropriate to the site context. As part of design workshop, community stakeholders and City staff and officials were invited to review and provide feedback on the initial redevelopment concepts.

Three redevelopment concepts emerged from the design workshop, each with a distinct land use scheme: (1) continued light-industrial uses, (2) corporate campus, and (3) mixed residential and commercial redevelopment. Feedback was solicited from stakeholders, local real estate developers, the Planning Commission, and City Council at multiple meetings.

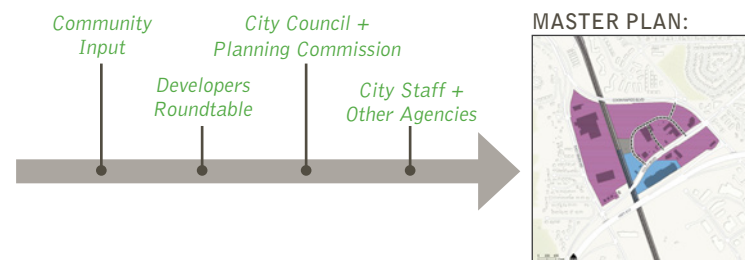
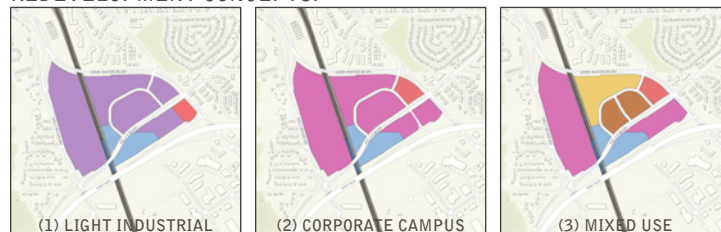
The resulting preferred redevelopment master plan is presented in [Chapter 3. Master Plan](#). The plan illustrates potential land use, roadways access, and bike and pedestrian improvements.

### Implementation Strategies

Key strategies and resources were also identified to support implementation of the master plan. [Chapter 4. Implementation](#) of this report delineates key public infrastructure improvements. Also included in this chapter are potential funding strategies and resources, as well as recommendations for zoning and regulatory controls that will support future redevelopment.


*Three redevelopment concepts were developed through a design workshop and community open house process and eventually refined into the master plan.*

#### REDEVELOPMENT CONCEPTS:









*Understanding existing land uses, infrastructure, and transit investments in the station area is a critical component of the station area planning process. The analysis in this chapter provides a baseline understanding of conditions on the ground today and identifies key constraints and opportunities for enhancing the station area, supporting transit use, and increasing redevelopment potential.*

## 2. **EXISTING CONDITIONS**



# 2. Existing Conditions

## LAND USE



*Vacant / underutilized areas*



*Diversified Adjustment Services on Coon Rapids Boulevard*

The existing study area, the area bounded by Highway 610, East River Road, and Coon Rapids Boulevard, is approximately 100 acres in size and includes over 430,000 square feet of industrial uses and 22,000 square feet of retail.

### Industrial

Land uses in the station area are predominantly industrial and light industrial, featuring low density, single-story buildings set back from the road. Within the study area, existing businesses support an estimated 600 jobs, including employers such as John Roberts Company, Kurt Manufacturing, and A-1 Engineering. The southeast portion of the station area falls within Port Evergreen, one of four redevelopment areas along Coon Rapids Boulevard targeted for office, commercial, and higher density residential development. Evergreen Industrial Park (located south of Highway 610 and west of Coon Rapids Boulevard) is an employment center with more than 3,000 jobs.

### Commercial / Institutional

A small number of commercial/institutional uses are located around Coon Rapids Boulevard. On the west side of Coon Rapids Boulevard is Diversified Adjustment Services, Anoka County Community Action Program's Head Start and Heritage Auto Body. On the east side of Coon Rapids Boulevard are Ace Hardware, Culver's, and Holiday Gas Station.

### Residential

A small number of homes remain within the study area located on the north side of Foley Boulevard. Recognizing

that residential uses in an industrial area are not ideal from a land use perspective, the City has been acquiring homes for redevelopment as they become available from willing owners. The study area is bordered by residential neighborhoods to the north, east, and west. Most of this area features single-family detached housing, with the exception of areas to the north and east across Coon Rapids Boulevard which include clusters of single-family attached and multi-family housing. Although proximate to the Foley Boulevard station, these neighborhoods are not well connected physically or visually to the study area.

### Parks and Open Space

Two local parks are located within the station's half-mile radius, Parkside Park and Mason Park. Coon Rapids Dam Regional Park, located west/northwest of the station area, is a popular destination for recreation with views of the Mississippi River and access to regional trails.

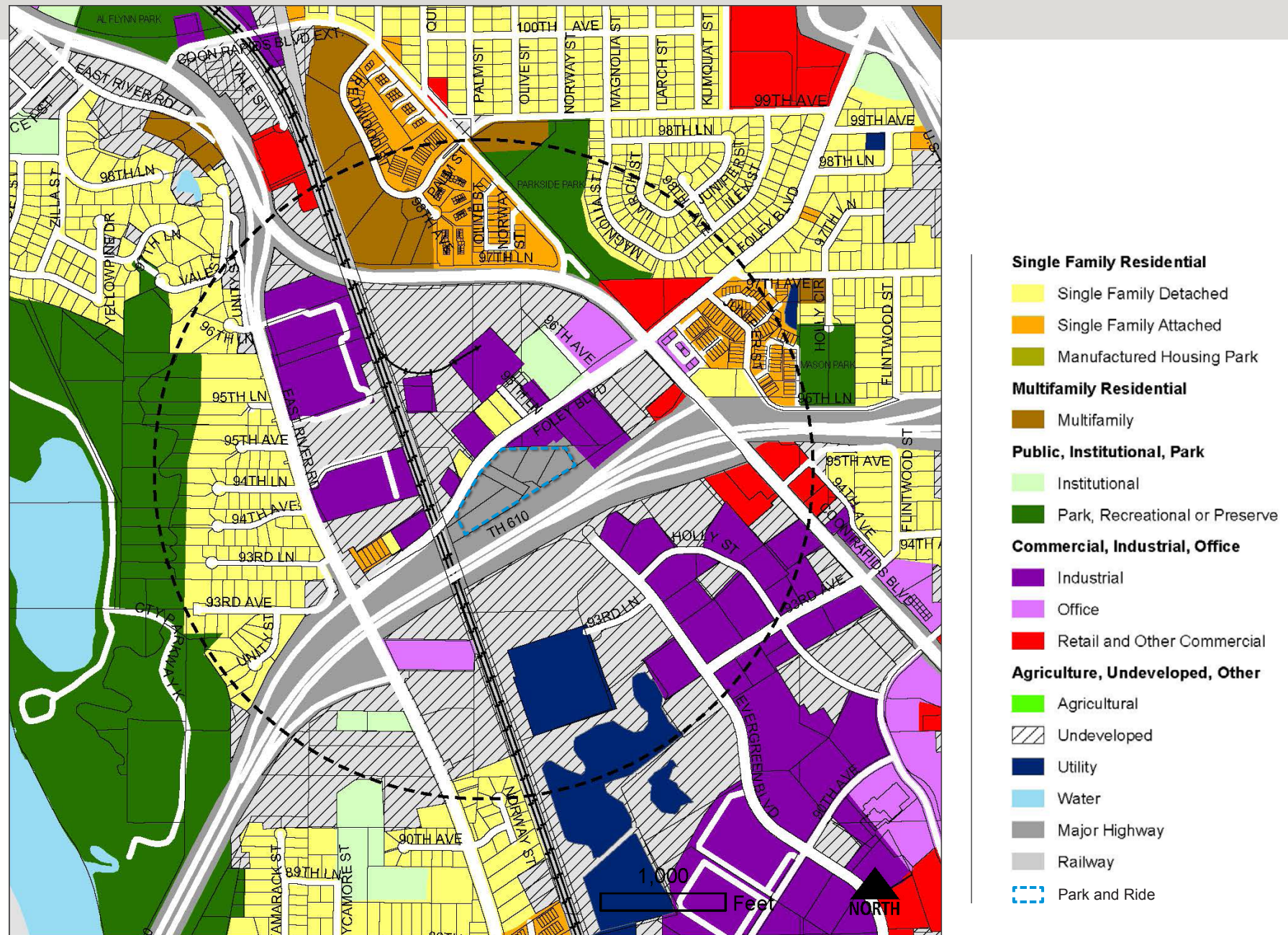
### Transportation

An existing rail corridor runs north-south through the station area, carrying about 100 freight, 2 Amtrak, and 10 Northstar Commuter Rail trains per day. There are no rail station stops at Foley today. The existing Metro Transit Park and Ride occupies an eight acre site on the south side of Foley Boulevard.

### Vacant / Underutilized

The study area includes large undeveloped areas which are primarily owned by adjacent businesses and/or encumbered by wetlands. Anoka County owns several vacant/underutilized parcels on the north side of Foley just east of the rail.

Figure 2.1 - Existing Land Use





## ROADWAYS

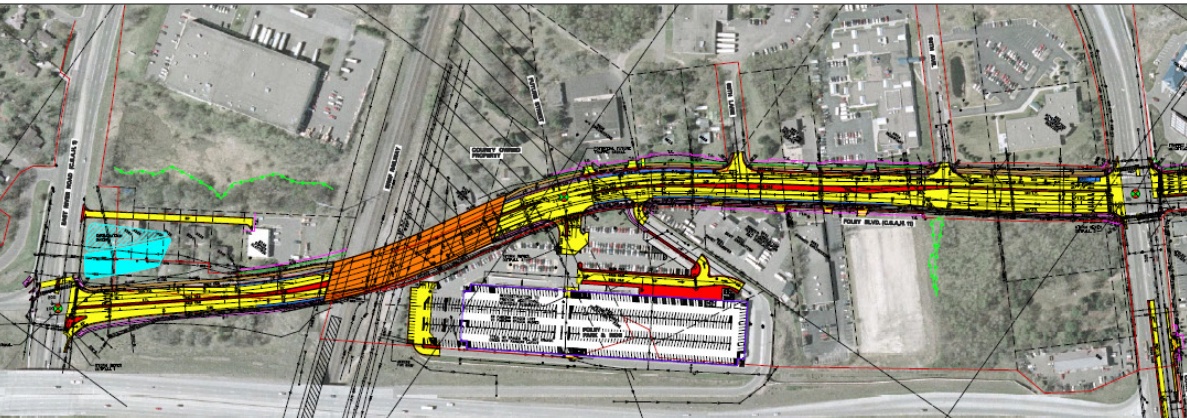


Existing at-grade rail crossing at  
Foley Boulevard

The roadway network in the study area is limited, with only one through road (Foley Boulevard). This superblock pattern along with the rail right of way creates challenges to connectivity and limit access to the station from the surrounding neighborhoods.

# Foley Boulevard Reconstruction

Anoka County has plans for the reconstruction of Foley Boulevard from East River Road to Coon Rapids Boulevard. The proposed design includes bridging Foley over the railroad tracks, a new signal near the Park and Ride, and sidewalk and trail along Foley. The proposed bridge over the rail tracks accommodates space for a potential transit platform and pedestrian connection under the bridge. While the proposed reconstruction is seen as a critical safety improvement over the existing at-grade rail crossing, a number of businesses along Foley have raised concerns about limited access resulting from the proposed median that will extend much of the length of the reconstruction area. The project is currently awaiting funding.



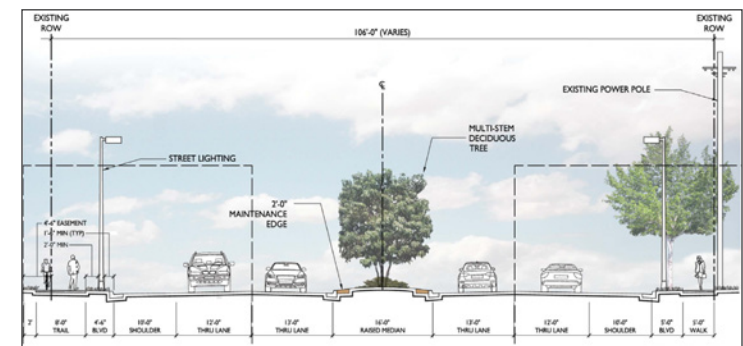
*The planned reconstruction of Foley Boulevard will elevate Foley over the rail tracks and accommodate spacing for a future station platform and/or pedestrian connection under the bridge.*

## Highway 610

Extending through the study area, Highway 610 provides regional connectivity west to 35W, east to County Road 81/Interstate 94, and south to downtown Minneapolis through Highway 252. Accessibility is currently limited in the study area as only the westbound ramps have been constructed. While improved connectivity is desired by the City, Anoka County, and MnDOT, access spacing with other interchanges, the BNSF Railroad, and the nearby Mississippi River have prevented a specific interchange design from being identified.

### East River Road & Coon Rapids Boulevard (CSAH 1 & 3)

East River Road and Coon Rapids Boulevard are minor arterials that extend northwest to southeast through the study area. The Coon Rapids Boulevard/East River Road Corridor Study was completed in 2010 to identify concepts for improving mobility, increasing safety, and enhancing the corridor's appearance and economic vitality. The cross section proposed through the study area include a landscaped center median with two through lanes, a bus lane/shoulder and trails/sidewalks. The study also recommended corridor elements such as lighting, crosswalk markings, boulevard and median trees, shrub/perennial plantings, etc.



*Illustrative Street Section from the East River Road Corridor Study*

Figure 2.2 - Existing Roadways - Annual Average Daily Traffic (AADT)



## Annual Average Daily Traffic (AADT)

- 0 - 2,850
- 2,851 - 10,300
- 10,301 - 24,300
- 24,301 - 52,000
- 52,001 - 94,000

Source: MnDOT, 2012



## SIDEWALK &amp; TRAIL



*Auto-oriented environment in the station area today*



*Existing sidewalks and trails in the station area do not connect to nearby parks and regional trailways.*

The existing sidewalk and trail network is limited and does not support walking or biking to the station area. Figure 2.3 shows the existing and proposed sidewalks and trails in the area. Numerous gaps exist today in the sidewalk network around the station, and many of the existing sidewalks are in need of repair.

Key issues and opportunities include:

- Incomplete sidewalk/trail connections along Foley Boulevard, East River Road, and Coon Rapids Boulevard to surrounding residential neighborhoods, parks, and employment areas.
- Need for more direct trail connection to Evergreen Industrial Park from the Park and Ride; potential to connect along the east side of the rail corridor under Foley Boulevard.
- Need for trail connections along Foley Boulevard connecting to the Coon Rapids Dam Regional Park and Mississippi River Regional Trail.
- Proposed grade separation of Foley Boulevard over the rail corridor (which includes new sidewalk and trail along Foley) will enhance bike and pedestrian safety and improve access to the station.

While proposed sidewalk and trail improvements along with the Foley Boulevard reconstruction will help to fill gaps in the bike and pedestrian network, further streetscape, sidewalk, and trail enhancements are needed to encourage a truly walkable and bikeable TOD environment.



*Existing sidewalk and trail network is incomplete and in poor conditions in some locations. The lack of street trees, wayfinding, landscaping, lighting, and dedicated bike facilities further inhibits walking and biking in the station area.*

Figure 2.3 - Existing Sidewalk &amp; Trail





## TRANSIT



*Metro Transit Express Bus service connects the Foley Park and Ride with destinations such as Downtown Minneapolis and Northtown Mall.*



*While a Northstar commuter rail stop has been considered previously for Foley Boulevard, there are no current plans for a stop at this location.*

### Existing Express Bus / Park & Ride

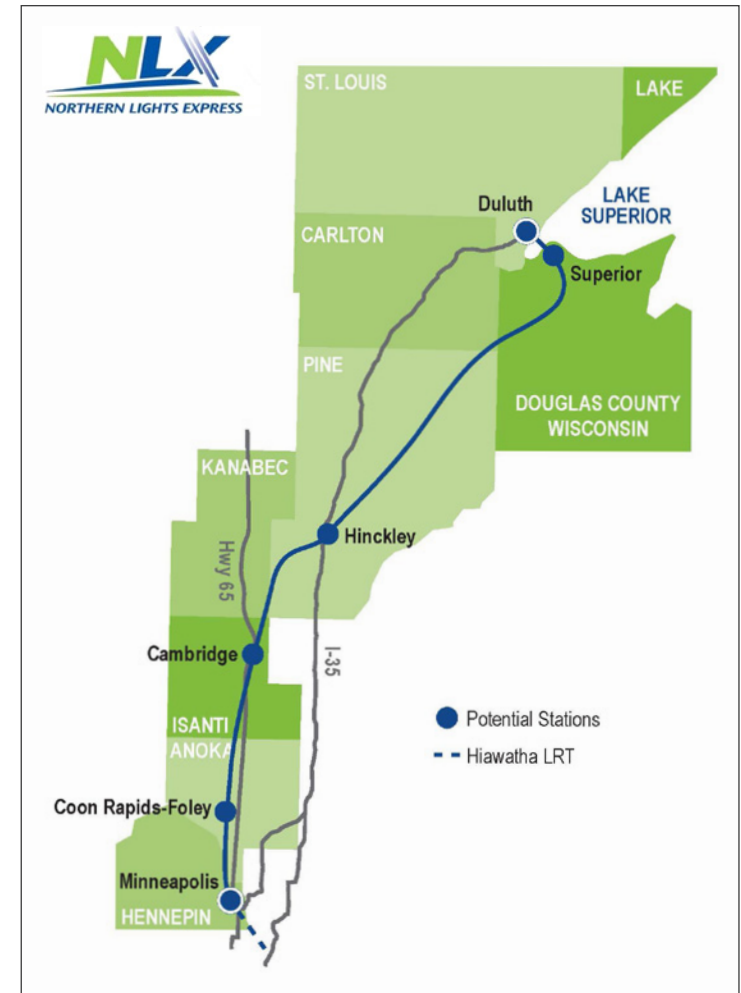
Metro Transit express bus service provides frequent, all-day service from the Foley Boulevard station to downtown Minneapolis (Routes 850 and 852). This is currently the fastest transit option from the area to downtown Minneapolis. Users value the frequency of service (2-3 buses per hour), relatively short ride time (approximately 30 minutes), and lower cost in comparison to commuter rail. The Foley Park and Ride is well-utilized and is one of the largest in the system. The existing Park and Ride (parking lot and ramp) accommodates 1,234 cars and is currently at full capacity (95% utilization). The existing parking ramp cannot take on additional decks, and there are no current plans for expansion.

### Potential Transit Expansion

#### NLX Express

The proposed Northern Lights Express (NLX) high-speed route to Duluth heads north out of the station area following the Burlington Northern Santa Fe (BNSF) rail corridor. NLX will connect from Target Field Station in Minneapolis to the City of Duluth, serving a projected 2,500 riders daily. The proposed route includes a north metro station at Foley Boulevard.

BNSF currently uses this railway for intermodal transport, BNSF bulk and merchandise trains, Canadian Pacific and Union Pacific Railroad, and Amtrak trains. BNSF would grant an easement to MnDOT for operation of passenger rail services. Recent feasibility studies suggest that the addition of a transit station at Foley Boulevard (either NLX or Northstar) will require adding a third main line track through the area to accommodate crossovers between the platform and the Coon Creek junction north of the station.



*The Northern Lights Express is a proposed high speed passenger rail project that would connect downtown Minneapolis to Duluth. If constructed, NLX will operate on approximately 155 miles of existing BNSF rail corridor and include a stop at Foley Boulevard.*

Station planning for NLX is currently underway to determine track and station location and design. Preliminary designs identify parking and a station building on the north side of Foley, east of the rail.

### ***Northstar Commuter Rail***


The possibility of a Northstar station stop at Foley Boulevard has been explored previously, but not pursued based on a number of factors, including impacts to trip time for the line; proximity to nearby stations; the success and frequency of the existing express bus service at Foley; and lower than expected ridership at existing Northstar stations. Commuter rail, while more attractive to many riders because of the quality of experience (wi-fi, dimmed lights, etc.), may also have a hard time competing with express bus at Foley, given shorter ride times to downtown, more frequent service, and the lower fare costs by bus.



*While a stop for the Northstar Commuter Rail has been considered at Foley, it has not been pursued given the added trip time, proximity of nearby stations, and the frequency and success of the existing express bus service at the Foley Park and Ride.*







*The Master Plan for the Foley Boulevard Station represents a long-term vision for the area. Change will not occur overnight and numerous public and private steps will need to occur to achieve the vision. The Master Plan is shaped by a number of factors, including existing conditions (particularly those conditions that are unlikely to change over time); planned infrastructure improvements (such as the Foley Boulevard reconstruction); and the guiding principles developed through the planning process.*

### 3. **MASTER PLAN**



# 3. Master Plan

## GUIDING PRINCIPLES

*The Guiding Principles for the Foley Boulevard Station Area embody the community's general desires and objectives for future change, investment, and redevelopment within the area. The Guiding Principles define the big picture direction and character for future public and private investment in the area. They are also intended to be touchstones for policies, plans, and future decision-making related to the Foley Boulevard Station Area.*

1

*Create an employment area that provides a mixture of industrial, service, retail, and transit uses.*

2

*Maximize the area's employment potential by supporting revitalization and increased employment densities.*

3

*Phase redevelopment to support gradual transition and collaboration with property owners.*

4

*Support access to transit and employment uses by improving connections to adjacent neighborhoods.*







## AUTO ACCESS PLAN

*The diagrams shown on the following pages illustrate the key components and concepts that comprise the Station Area Master Plan. These Master Plan diagrams provide high-level descriptions of the desired future conditions for automobile access, land use, and pedestrian/bicycle connectivity in the station area.*

*Also included in this section are descriptions and images of the desired character for the station area with regard to various land uses, transit facilities, and streetscape design.*

The Auto Access component of the Master Plan demonstrates how street access and circulation can be improved within the area to transition the existing “superblock” pattern to a smaller, more connected block pattern that will increase the area’s redevelopment potential and improve safety and accessibility for all modes of transportation.

### EXISTING ROADWAYS AND INTERSECTIONS

The three Anoka County roads that bound the study area- Foley Boulevard, East River Road, and Coon Rapids Boulevard- will continue to function as minor arterial roads, providing through connections in the community and access to the regional transportation network.

### FOLEY RECONSTRUCTION

The Auto Access Plan shown in Figure 3.1 reflects Anoka County’s current reconstruction plan for Foley Boulevard. The planned reconstruction, which includes a new median, controlled intersections, and a bridge over the railroad tracks, has been identified by the County as a safety priority. With more than 100 trains a day crossing Foley, the proposed bridge will not only improve safety, but shorten train travel time through the area. Per the County design, a median will be installed along Foley Boulevard almost the entire length from East River Roads to Coon Rapids Boulevard. While the median is necessitated by County roadway standards, it also reduces the number of full access intersections, preventing left turns in or out of most properties. The City should continue to work with Anoka County to identify one or more locations for median breaks that can provide access for properties.

### NEW ROADS

The Auto Access Plan addresses the connectivity issues created by the generally limited road network that exists in the area today - as well as the proposed Foley median - by introducing new roads north of Foley and east of the rail. These new

roads will encourage redevelopment by creating access to undeveloped areas and providing connections to otherwise hard-to-access sites. The new loop road is proposed on the north side of Foley Boulevard, connecting from the proposed signalized intersection at the Park and Ride to what is today an access road between the Diversified and Headstart properties. A second road segment is proposed to connect the loop road to Coon Rapids Boulevard. While the new intersection at Coon Rapids Boulevard will only be right-in/right-out, it will provide a valuable outlet for sites on the north side of Foley, particularly those penned in by the proposed Foley median. This roadway connection will also provide travellers heading south on Coon Rapids Boulevard with an alternate route to the station. A third new road segment, created by extending 95th Lane to the loop road further breaks down the block pattern and improves connectivity.

### HIGHWAY 610 INTERCHANGE

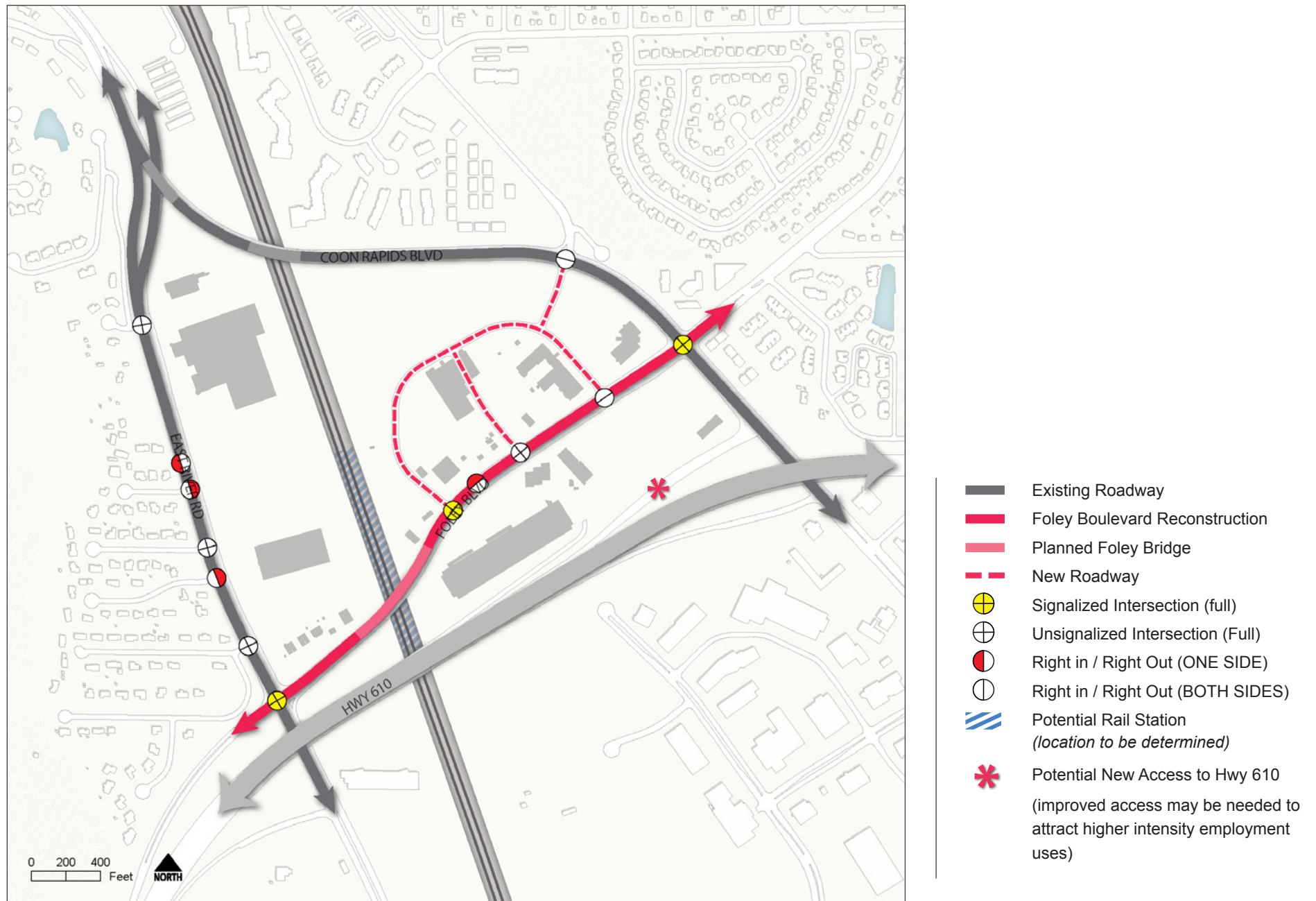
The lack of a full interchange at Highway 610 is a barrier for some businesses that would otherwise be interested in locating in the station area. The City should continue to explore with Anoka County and MnDOT how the eastbound access to Highway 610 could be provided.

### TRANSIT STATION / PARK AND RIDE

The proposed roadway network provides for safe and convenient access to the existing Park and Ride/express bus station, which is expected to be the primary form of transit for now. While it is unlikely that a Northstar Station will be built in the near-term, redevelopment should not preclude the long-term development of a Northern Lights Express (NLX) and/or Northstar Station. Key considerations include:

- Maintaining space under the proposed Foley bridge for a potential rail station platform and/or pedestrian access.
- Accommodating multi-modal transfers, such as buses and taxis, at the station.

Figure 3.1 - Auto Access Plan



Existing building footprints are also shown to illustrate the new roadway locations relative to existing development.

## LAND USE PLAN



*The Land Use Plan supports continuation of existing light industrial uses, but allows for diversification of uses over time.*



*Additional retail and services in the station area will support the day to day needs of employees, residents, and commuters alike.*

The Land Use Plan for the Foley Boulevard Station Area focuses on strengthening this area as an employment district for the community. As in many developed communities, the City of Coon Rapids has a limited number of areas for industrial, office, service, and retail uses. Continued use of this area for such employment uses is not only desired, but appropriate given that existing land use context in this area is not conducive to residential development (i.e. The presence of railroad tracks with more than 100 trains daily; the physical separation of this area from other residential neighborhoods in the City; and the limited potential for a future commuter rail in this area).

This plan anticipates that the study area will include a mix of light industrial, service, office, and retail uses. As existing buildings reach the end of their useful life, there may be opportunities to diversify the types of uses to include office/flex/showroom space or corporate headquarters. The area may also incorporate more service and retail uses that can serve transit users, as well as employees of area businesses.

Adequate site access will be important to supporting the health of businesses in the station area. With new medians proposed as part of the Foley Boulevard reconstruction, access to existing and future businesses along Foley will be limited. The City should work with Anoka County to explore options for median breaks to provide access, particularly for those properties on the south side of Foley. The City, Anoka County, Metro Transit, and property owners should also collaborate on how future redevelopment south of Foley could be designed to create access from those properties to the proposed signalized intersection at the Park and Ride.



*There is long-term potential to redevelop existing light industrial areas along East River Road into office/flex buildings or corporate headquarters.*

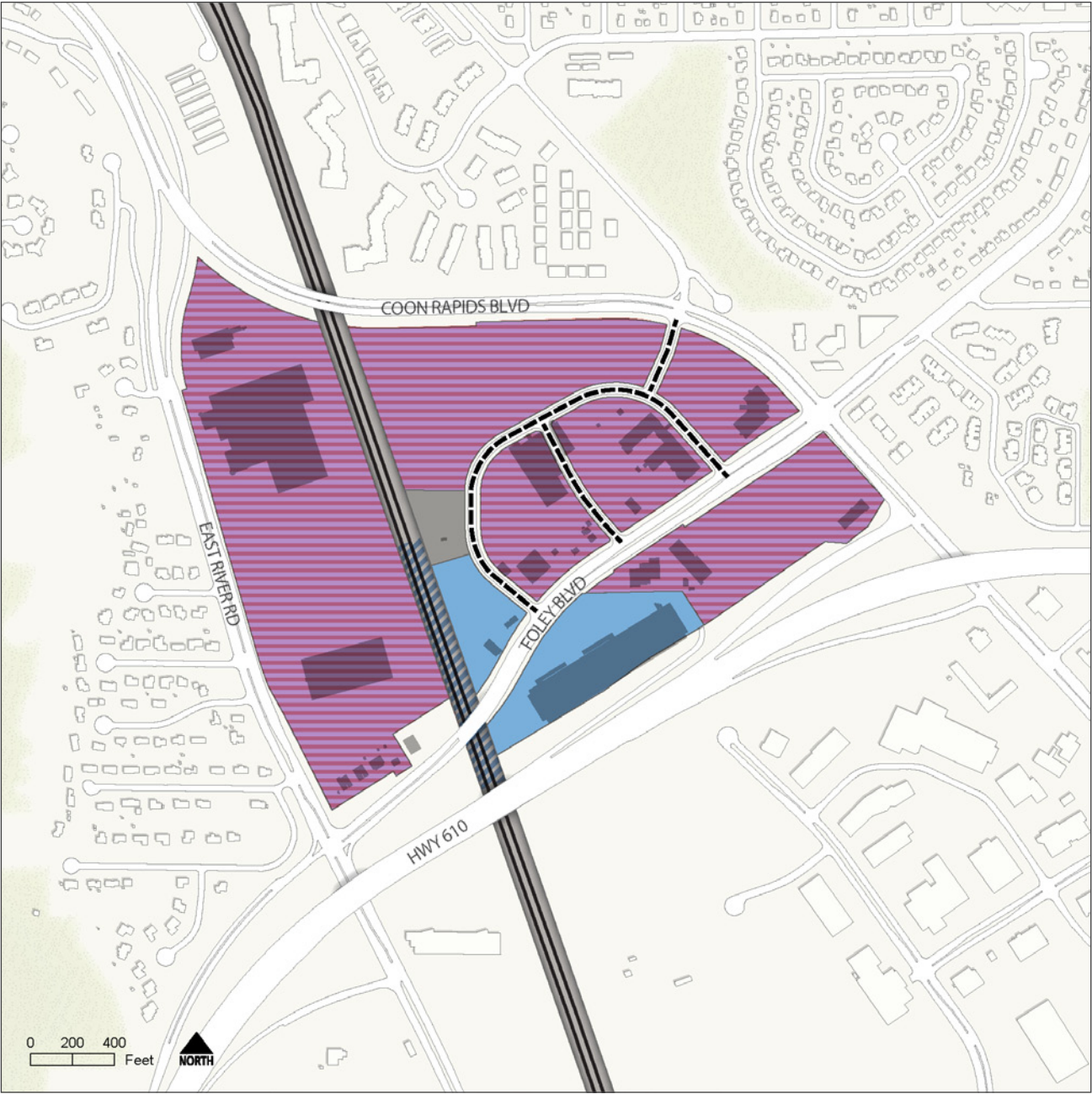
In addition to providing an attractive location for businesses, high quality building and site design in the Foley Boulevard Station Area will promote a more walkable and transit-supportive environment.

#### **Character and Form Considerations:**

- Long-term potential for intensification of uses beyond what exists today (i.e. two- to three-story buildings).
- Primary entrances to buildings should face onto the public street; this may require two-sided buildings with access from both the street side and parking lot.
- Sidewalks should connect building entrances to the public street to encourage walking and biking to businesses.
- Integrate stormwater features to serve as an open space amenity for employees and to enhance the public realm.
- Encourage use of high quality building materials, landscaping, and site design to contribute to a more visually appealing and engaging environment.



Figure 3.2 - Land Use Plan



Development Summary

Land Use	Acres*	Yield
Employment	77	1,350,000 sq. ft.
Transit	10	
Utility	2	
Right of Way	10	
Total	99	

\*Net acres excludes wetlands that may be available for development through mitigation.

- New Roadway
- Employment (Light Industrial, Office, or Retail)
- Transit
- Utility
- Potential Rail Station (location to be determined)
- Potential New Access to Hwy 610 (improved access may be needed to attract higher intensity employment uses)

## LAND USE PLAN

*Appropriate site design, landscaping, and open space amenities contribute to a more attractive and walkable environment for employees, shoppers, and commuters accessing the station.*



*Commercial retail and office buildings should be located closer to the public street with parking behind.*



*High quality building design and landscaping contribute to a more walkable, pedestrian-friendly environment.*



*Stormwater features can serve as open space amenities for employees.*



*A community gathering space at the transit station supports the needs of transit riders and creates a welcoming atmosphere.*



## TRANSIT

Areas immediately east side of the railroad tracks on either side of Foley Boulevard are designated for existing and future transit facilities. This includes the existing Park and Ride, as well as room for additional parking and a potential transit plaza and station building on the north side of Foley should NLX or Northstar add a station at this location.

### **Character and Form Considerations:**

- Bike facilities should be integrated into the transit station (bike parking, repair/pump station, bike racks, etc.)
- Station waiting areas should include seating, landscaping, shelters, public art, and lighting.
- Develop a transit plaza and/or station building that can serve as a community gathering space with hardscape surfaces, public art, seating, lighting, and other amenities.



*Transit waiting areas around the existing or future transit station should incorporate amenities like lighting, seating, shade, and public art.*

## UTILITIES

The Land Use Plan assumes that the existing Connexus Energy building and equipment will remain as these facilities are not easily moved. The cellular tower may be relocated in the future, depending on proposed redevelopment.

### **Character and Form Considerations:**

- As streets are reconstructed, existing above ground utilities should be relocated below ground within the public street rights-of-way whenever feasible.
- Enhance the visual aesthetics of any above ground utility structures with landscaping, fencing, or other approved screening devices.
- Integrate public art into the design of street and utility infrastructure.



*Bike lockers, parking, and repair stations should be located at or near the station to encourage biking to transit.*



*Integrate artistic elements into the design of street and utility infrastructure.*



## BIKE AND PEDESTRIAN ACCESS PLAN



*A well-connected system of sidewalks and trails in the station area encourages residents to walk and bike to transit and other amenities, and contributes to a more vibrant, safe, and active public realm.*



*The Green Line's West Bank Station shows how vertical circulation may use a bridge to provide access.*

A pedestrian and bike-friendly environment ensures safe routes to the station and access to and from key residential, commercial, and recreational destinations. Such improvements are critical to promoting transit use and active transportation both within the study area and for the surrounding neighborhoods. The Bike and Pedestrian Access Plan (shown in Figure 3.3) significantly expands the non-motorized transportation network by proposing new sidewalks and multi-use trails that fill critical system gaps and connect to key amenities and destinations in the area.

### FOLEY RECONSTRUCTION

In keeping with Anoka County's plans for the reconstruction of Foley Boulevard, the Bike and Pedestrian Access Plan shows a trail and sidewalk along Foley, including along the proposed bridge over the rail corridor. The County's design for the bridge accommodates a potential pedestrian and bicycle connection under the bridge to connect transit services to development on either side of Foley. The concept at right also shows potential locations for vertical circulation on the bridge to connect the sidewalk/trail above to a future rail transit platform below, should that develop over time.

While the County plans do not currently include streetscape elements like boulevard and median trees along Foley, landscaping and other pedestrian-friendly streetscape features should be integrated into the public right-of-way on Foley where possible. Additionally, where boulevard and median widths do not allow for significant planting, the City should work with private developers to encourage additional lighting and landscaping within the private right-of-way along Foley. See page 33 for an illustrative street section of Foley Boulevard.

### NEW ROADS

Pedestrian facilities are proposed for all new roads within the study area to create more direct connections to the station from Coon Rapids Boulevard and increase pedestrian access to and within the sites north of Foley. An illustrative street section is shown on page 34 for the new roadway segment.

### COMMUNITY CONNECTIONS

In addition to improving bike and pedestrian access near the station, the Future Bicycle and Pedestrian Access plan connects the station area and potential future residents and employees to the amenities in the broader Coon Rapids community. This includes trail and sidewalk connections west to the Mississippi River and Coon Rapids Dam Regional Park, south to Evergreen Industrial Park, and north to adjacent residential neighborhoods.

### STREETSCAPE AMENITIES AND CHARACTER

While sidewalk and trail improvements will go a long way to encouraging active transportation around the station, additional amenities should be considered to create a more walkable and bikeable environment. The illustrative streets sections discussed on pages 32-34 demonstrate the desired streetscape character, dimensions, and amenities.

Figure 3.3 - Bike / Pedestrian Access Plan





## BIKE AND PEDESTRIAN ACCESS PLAN



*Clear wayfinding and trail signage can reduce conflicts between bikes and pedestrians and direct travellers to the station and other amenities in the area.*

## ILLUSTRATIVE STREET SECTIONS

The following pages show detailed street sections, dimensions, and descriptions of key features for select roadways within the station area. These street sections are intended to express the design intent and key characteristics or features of each street type. Sections are shown for the Foley Boulevard reconstruction area, as well as the proposed new roadways north of Foley Boulevard. For East River Road and Coon Rapids Boulevard, the streetscape character and dimensions should conform to recommendations from the Coon Rapids Boulevard/East River Road Corridor Study completed in 2010.

The proposed Foley Boulevard section is based on the dimensional criteria from Anoka County's current reconstruction plans. The proposed section integrates additional lighting and landscaping recommendations.



*A well-designed and maintained streetscape contributes to a more inviting, walkable, and safe public realm in the station area. Features like street trees, public and private landscaping, pedestrian-scale lighting, as well as active ground-level uses that front the public street enhance the safety and comfort of pedestrians.*

## KEY STREETSCAPE ELEMENTS:



*Street trees and landscaping*



*Seating & Shade*



*Wayfinding*

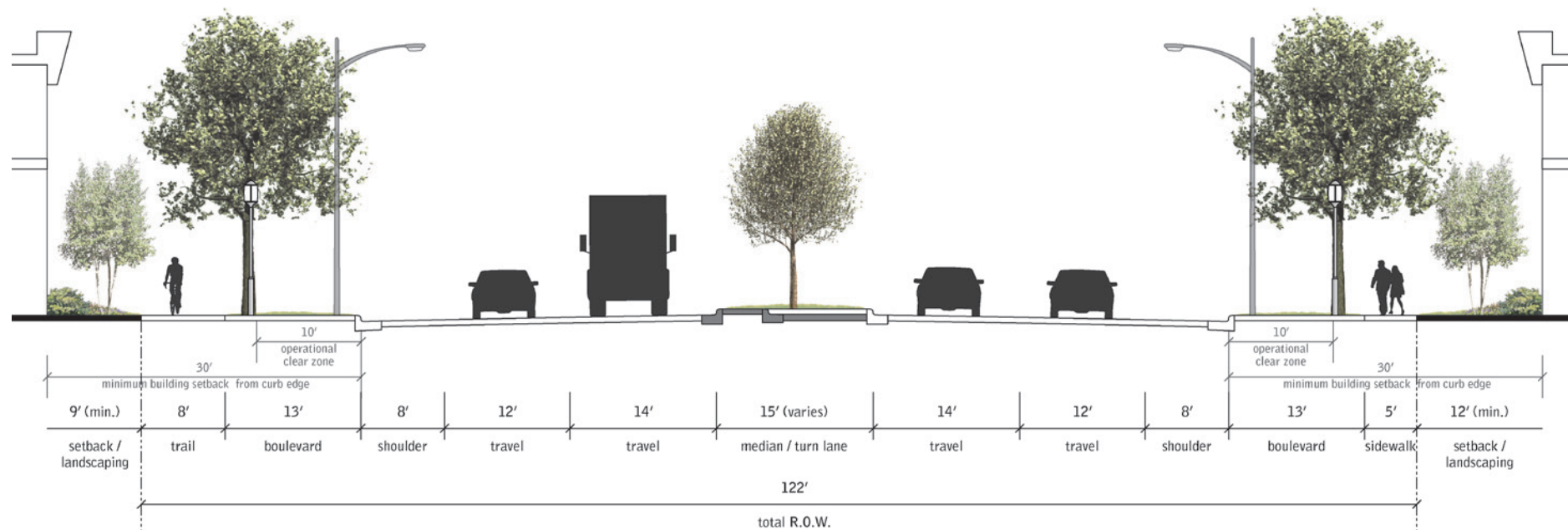


*Enhanced pedestrian crossings*



*Auto & pedestrian-scale lighting*

Figure 3.4 - Illustrative street section - Foley Boulevard



### FOLEY BOULEVARD

This street section applies to the portion of Foley Boulevard that runs through the station area from East River Road to Coon Rapids Boulevard. The section includes a sidewalk on the north side of the street and multi-use trail on the south side. Bikes and pedestrians are buffered from automobile traffic by landscaping and trees. Landscaping and other pedestrian-friendly streetscape features are integrated into the public right-of-way on Foley. The City should work with private developers to encourage additional lighting and landscaping within the setback areas along Foley.

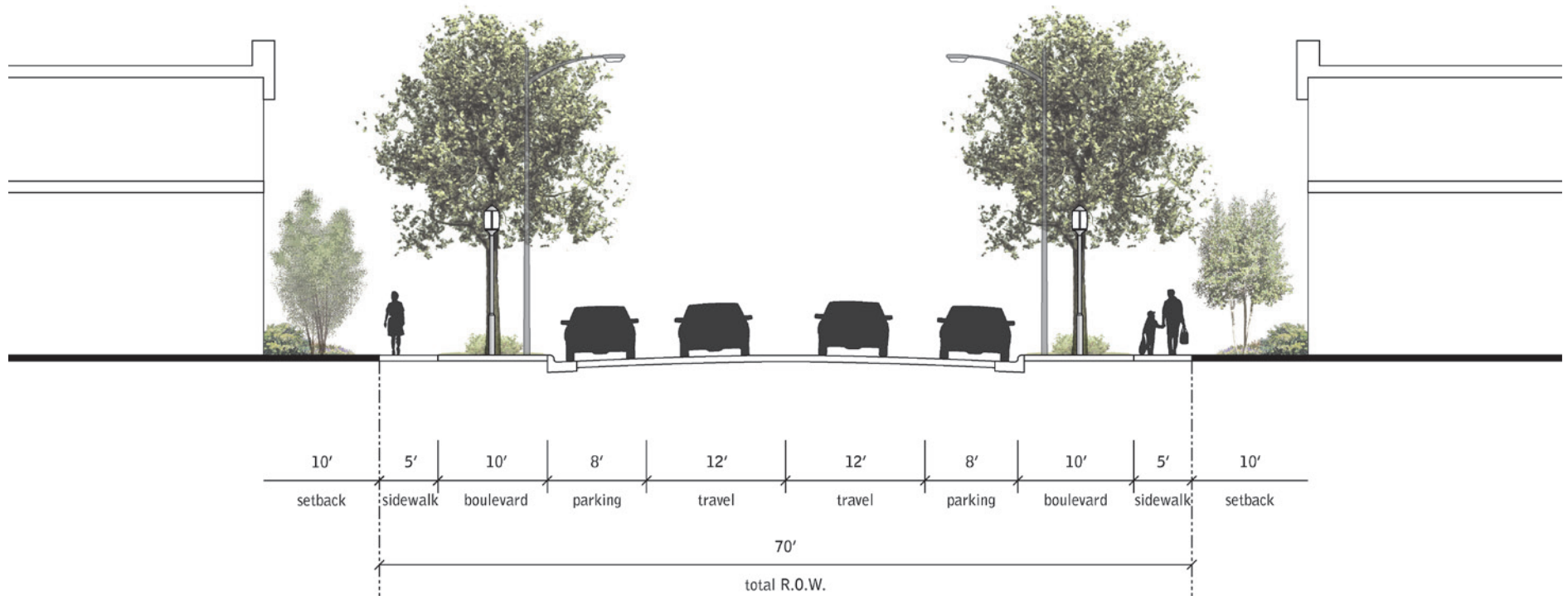
### Design Features:

- Sidewalk (north side of Foley)
- Trail (south side of Foley)
- Street trees on side boulevards (outside of the 10' operational clear zone)
- Street trees on median (where width allows)
- Street and pedestrian scale lighting
- Signage / wayfinding
- Enhanced intersections (street markings, special paving, countdown signals, ADA features)

### Dimensional Criteria

- Right-of-way width: 122' typical
- Pavement width: 83'
- Sidewalk: 5'
- Trail: 8'
- Boulevard: 13'
- Minimum building setback from curb: 30'

Figure 3.5 - Illustrative street section - New roadways (with sidewalks)



## NEW ROADWAYS

This street section applies to new roadways in the area north of Foley Boulevard and east of the rail. As shown in the section above, pedestrian-friendly street furnishings, street trees, landscaping, on-street parking, and lighting contribute to a more walkable, TOD-friendly environment. On-street parking is included on both sides of the street, and sidewalks on both sides of the street accommodate pedestrians.

## Design Features:

- Sidewalk (both sides)
- On-street parking (both sides)
- Street trees and planting
- Street and pedestrian scale lighting
- Signage / wayfinding
- Enhanced intersections (street markings, special paving, countdown signals, ADA features)


## Dimensional Criteria

- Right-of-way width: 70' typical
- Pavement width: 35'
- Drive lanes: 12'
- Sidewalk: 5'
- Parking lane: 8'
- Boulevard: 10'



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*Implementation is an essential component of the Foley Boulevard Station Area Plan. This chapter identifies the recommended actions for implementing the vision, guiding principles, and design characteristics.*

## **4. IMPLEMENTATION**



# 4. Implementation

## PUBLIC ACTIONS

*Redevelopment in the Foley Station Area will occur through a combination of public and private efforts. Public actions will generally consist of updating the City's regulations and policies and investments in public infrastructure. Private redevelopment of property will occur based on owner interest. Significant public investment in assembling and redeveloping properties in the study area are not anticipated.*

### RECOMMENDED UPDATES TO CITY POLICIES & REGULATIONS

A first step in implementing the Foley Boulevard Station Area Plan is updating the City policies and regulations that guide public and private investment in the study area. These include:

- 1) Comprehensive Plan
- 2) Zoning Map
- 3) Parks and Trails Plan

#### Comprehensive Plan

The future land uses designated in the 2030 Comprehensive Plan are shown in Figure 4.1. These include a combination of industrial, office, institutional, and commercial/mixed use that generally follow the existing land uses in the area.

The portion of the study area between the railroad tracks and Coon Rapids Boulevard also has a transit station overlay designation. The transit station overlay was created to specifically call out the long-term possibility of a transit station at Foley Boulevard and the need for transit-oriented development if that should occur. The Comprehensive Plan cites the need for the creation of a master plan to guide redevelopment.

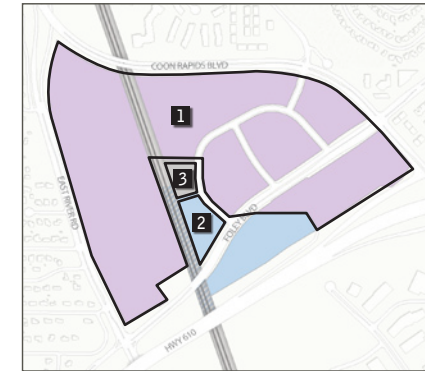
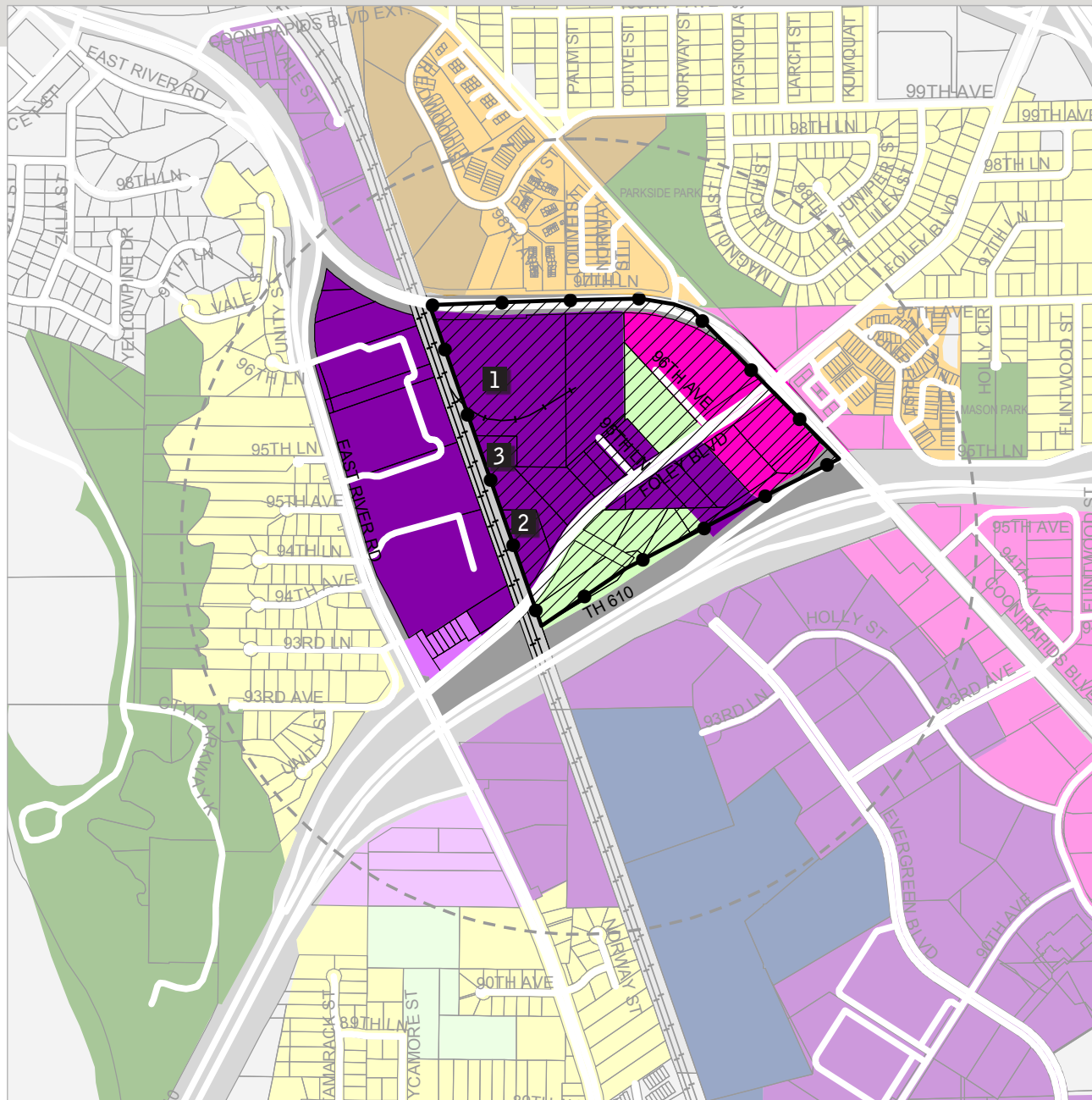
Changes to the Comprehensive Plan will be needed to support implementation of the Land Use Plan for the portions of the study area that are east of the railroad tracks. Given that a city-wide update to the Comprehensive Plan will begin in late 2015 or early 2016, it is recommended that reclassification of individual properties into new land use categories is delayed until that time. This will enable the City to better determine whether new categories should be created or if existing land use categories should be modified to address the flexibility in uses proposed in this plan. Examples of issues to be addressed include:

- 1** The absence of a land use category that can address the possibility of light industrial, service, office, or retail uses as is proposed for most of the study area.
- 2** The transit area on the north side of Foley Boulevard should be guided institutional similar to the existing Park and Ride.
- 3** The Connexus property should be guided utility as it needs to remain and cannot be redeveloped for other uses.

In the interim, it is recommended that the City modify the description of the Riverdale and Foley Commuter Rail Station Areas on page 2-23 to reference this station area plan as the master plan guiding development.



Figure 4.1 - 2030 Comprehensive Plan



*Comprehensive Plan change areas  
overlayed on the Future Land Use Plan*

#### Residential

- Low Density (2-4 dwelling units/acre)
- Moderate Density (4-7 du/a)
- High Density (7-30 du/a)
- Residential Mixed Use (7-30 du/a)
- Mobile Home (5-10 du/a)

#### Commercial

- Community
- Neighborhood
- Commercial Mixed use
- General
- Regional Shopping

#### Other

- Park, Recreation, and Preserve
- Office
- Industrial
- Institutional
- ROW
- Rail
- Water
- Utility
- Transit Station



## PUBLIC ACTIONS

**Zoning**

Existing zoning in the Foley Boulevard Station Area includes a combination of industrial, office, and PORT as shown in Figure 4.2. In addition, the portion of the station area along Coon Rapids Boulevard also has the River Rapids Overlay District.

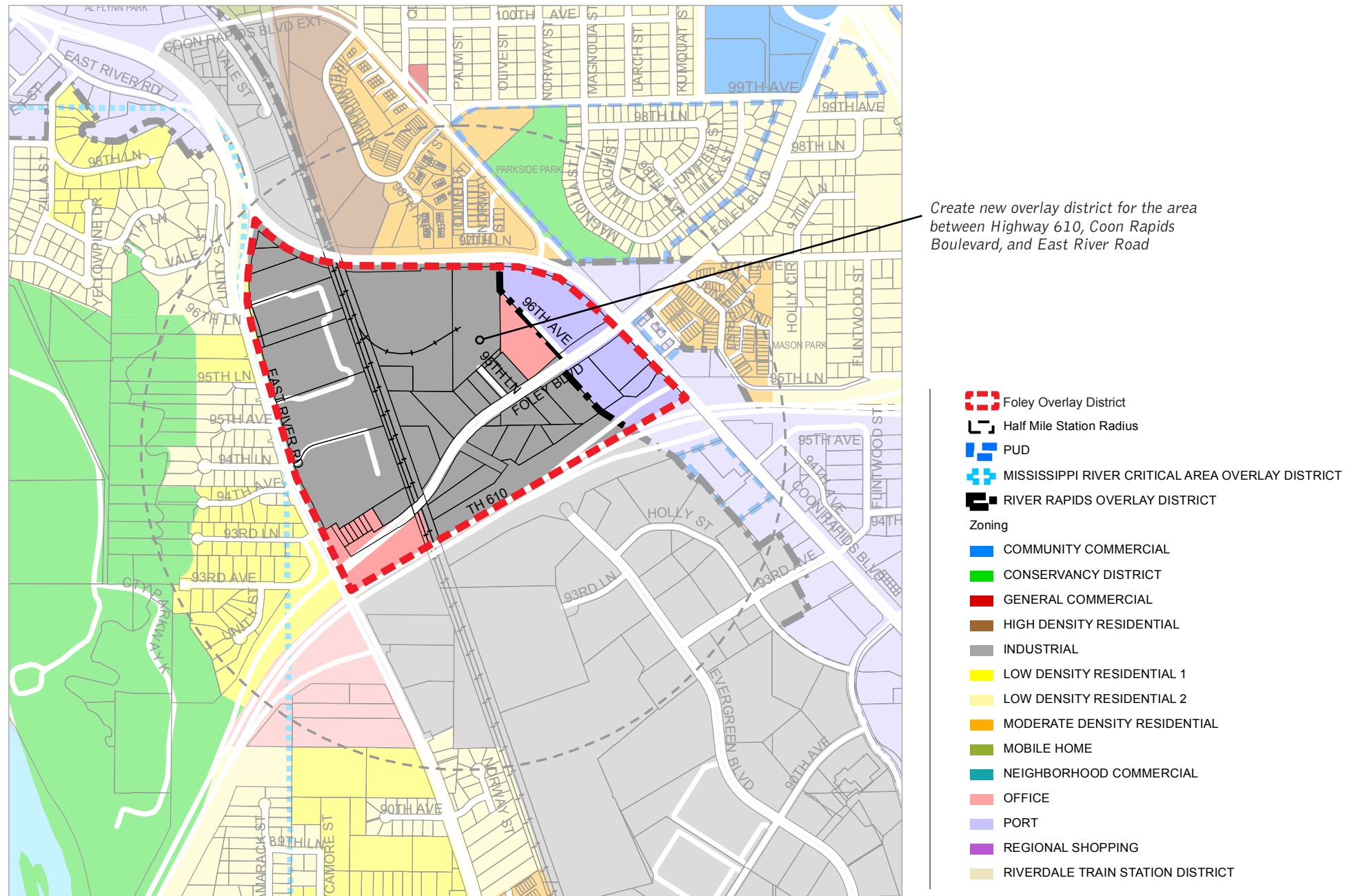
Given the physical separation of the study area from other commercial and industrial areas of the City due to Highway 610, it is recommended that both the PORT and River Rapids Overlay District designations should be removed.

The proposed flexibility for light industrial, service, office, and retail uses proposed in the Master Plan do not fit with the City's existing zoning districts. For example, most of the study area is currently zoned Industrial. This designation would not allow the development of retail along Foley Boulevard.

To provide more flexibility for redevelopment, the City should explore the creation of an overlay district for this area that would identify additional uses and design requirements that would be appropriate. The overlay district could allow the requirements of the underlying industrial district to continue to apply but provide alternative standards for those who are interested in pursuing uses not allowed in the industrial district. The overlay district could also provide for reduced requirements if specified design standards are met. Some examples of these provisions might be:

- Allowing stand alone retail or service uses with similar standards to the Community Commercial District or allowing retail or service uses as accessory uses as long as the use does not exceed a specified percentage of the building's overall use.
- Increasing the allowed lot coverage if the project has more than two stories.
- Decreasing setback requirements if specified architectural and site design standards are met such as if:
  - ▷ Parking is placed behind the building or there is shared parking with another site
  - ▷ Building design provides a consistent architectural treatment on all sides of a building
  - ▷ A courtyard, plaza, or seating area is incorporated into the development adjacent to a public street

Figure 4.2 - Existing Zoning



## PUBLIC ACTIONS

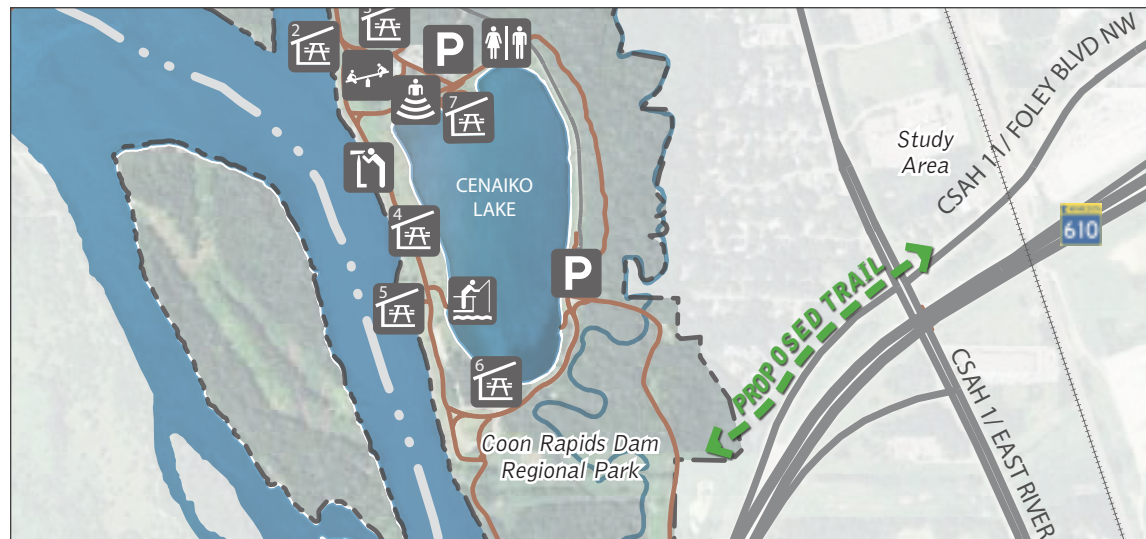
## Sidewalks and Trails

The Comprehensive Plan guides sidewalk and trail development in the Transportation Chapter of the Comprehensive Plan (Chapter 3). As shown in Figure 4.3, the Comprehensive Plan currently identifies the need for additional sidewalks along East River Road and Foley Boulevard. It also includes a sidewalk along Coon Rapids Boulevard Extension NW to connect into Parkside Park.

The Master Plan proposes a more connected network of sidewalks and trails to support non-motorized transportation and transit use. At a minimum this will involve the development of off-road sidewalks and/or trails on both sides of all of the local and arterial streets.

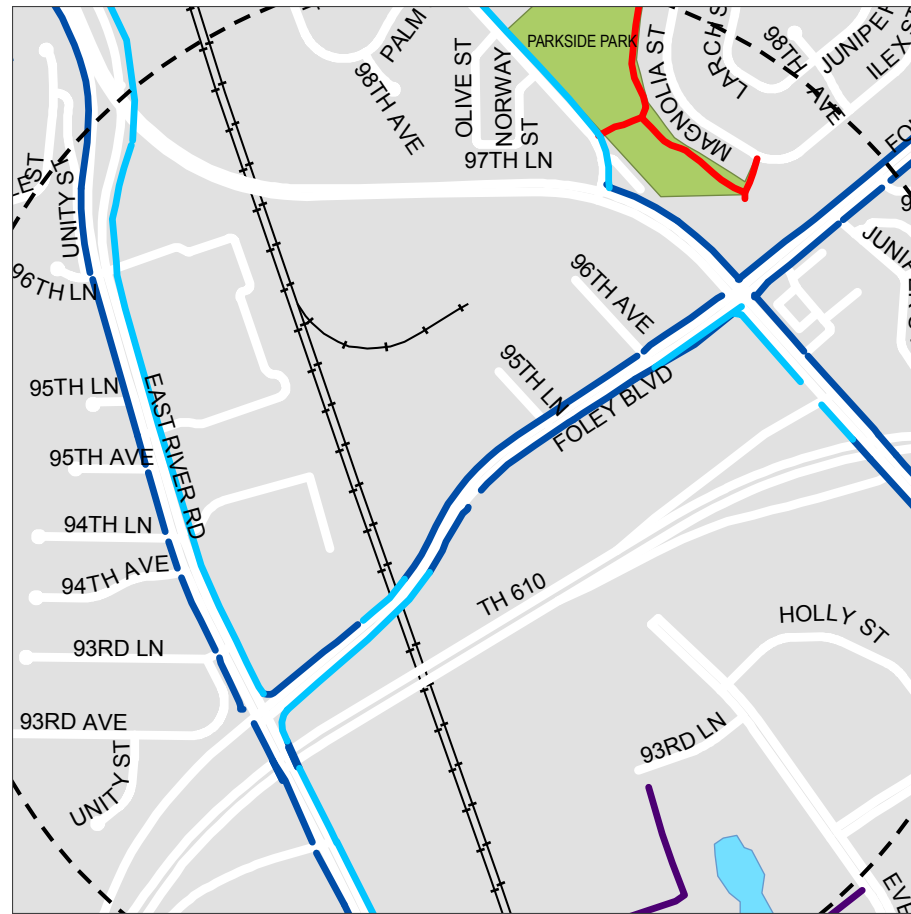
In addition to connectivity along street corridors, the plan proposes two new trails that will improve regional connectivity. The first extends west of East River Road to the Mississippi River and Coon Rapids Dam Regional Park. A recommended alignment for the City to explore is along the entrance ramp for Highway 610 and then through the existing west-east utility corridor to an existing paved Coon Rapids Dam Regional Park trail.

The second trail is a north-south trail along the east side of the rail corridor that will improve non-motorized connectivity to the transit facilities. The trail will link to existing residential neighborhoods and the Port Riverwalk redevelopment area to the north, and to Evergreen Industrial Park to the south, facilitating employee use of transit.



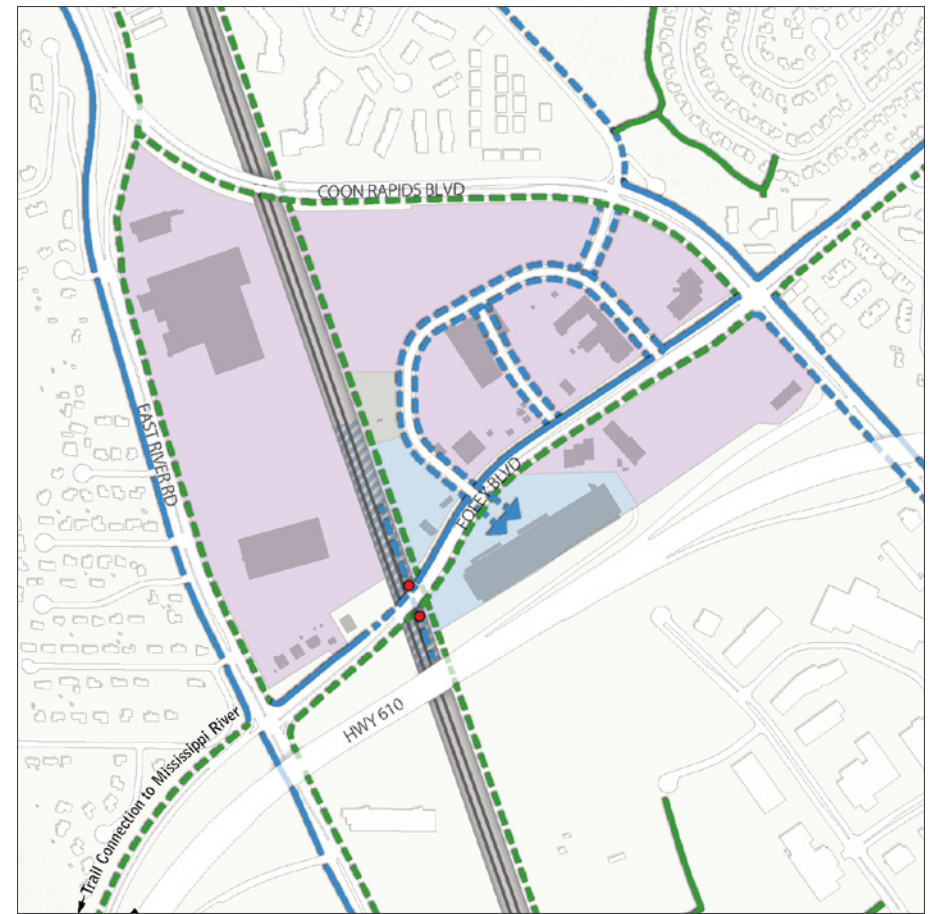
*The establishment of a trail from the study area to Coon Rapids Dam Regional Park will involve collaboration among the City, MnDOT, Anoka County Highway, and Anoka County Parks.*

Figure 4.3 - Existing Sidewalks and Trails



- InteriorParkTrailproposed
- InteriorParkTrail
- ProposedSidewalk
- Sidewalk

Figure 4.4 - Proposed Sidewalks and Trails



- Existing sidewalk
- Proposed sidewalk
- Existing Trail
- Proposed trail
- Vertical circulation
- Potential Rail Station  
(location to be determined)



## PUBLIC ACTIONS

*It is anticipated that redevelopment in the Foley Boulevard Station Area will primarily be the result of private initiative. However, given the number of planned public improvements for the area, it is important that the City of Coon Rapids continue to be proactive in ensuring public actions support the long term redevelopment of the study area and its transformation to a transit supportive area.*

## PUBLIC SECTOR PRIORITIES

**1 Influence Foley Boulevard Reconstruction**

Anoka County is planning to reconstruct Foley Boulevard to improve safety at the railroad tracks. The City should continue to work with Anoka County to enhance the roadway design to create a pedestrian environment with trees at regular intervals and within the median where possible, pedestrian scale lighting, the burying of the overhead electrical line, the design of the stormwater pond as an amenity, and the construction of trails and sidewalks. The City should continue discussions regarding the extent of median needed given its impact on local businesses.

**2 Create new loop road network**

The creation of the loop road network north of Foley Boulevard will open up areas for development, as well as reduce the long-term impacts of the construction of a median on Foley Boulevard. Given that the roads are located on what is currently private property, the City should initiate discussions with those property owners on the proposed plans. The City should also begin developing designs for the roadway.

**3 Explore access improvements for area south of Foley**

The reconstruction of Foley Boulevard with a center median will significantly impact access for properties on the south. A stoplight is proposed at the entrance to the Park and Ride and at the location of the loop road. Additional access points are not possible due to the short distance between the railroad tracks and Coon Rapids Boulevard. Discussion among the City, Anoka County, Metro Transit, and property owners should explore how development and redevelopment designs could accommodate the creation of a private, internal circulation road between commercial areas and the stoplight.

**4 Continue Exploration of Highway 610 Interchange**

The lack of a full interchange to Highway 610 may be a barrier for some businesses looking at the station area. The City

should continue to explore how the eastbound connections on Highway 610 can be made.

**5 Construction of trails and sidewalks**

Trails and sidewalks are fundamental components of creating a transit-oriented district. In addition to planning for sidewalks along local city streets, the City will need to collaborate with MnDOT, Anoka County and other agencies on the trail segments that extend beyond the study area. This includes the use of Highway 610 right-of-way to establish a connection to Coon Rapids Dam Regional Park. It also involves the acquisition of an easement or right-of-way along the railroad tracks for a trail that extends north-south from the study area to Port Riverwalk and Evergreen Industrial Park.

**6 Coordination with Northern Lights Express (NLX) and Anoka County on Station Area**

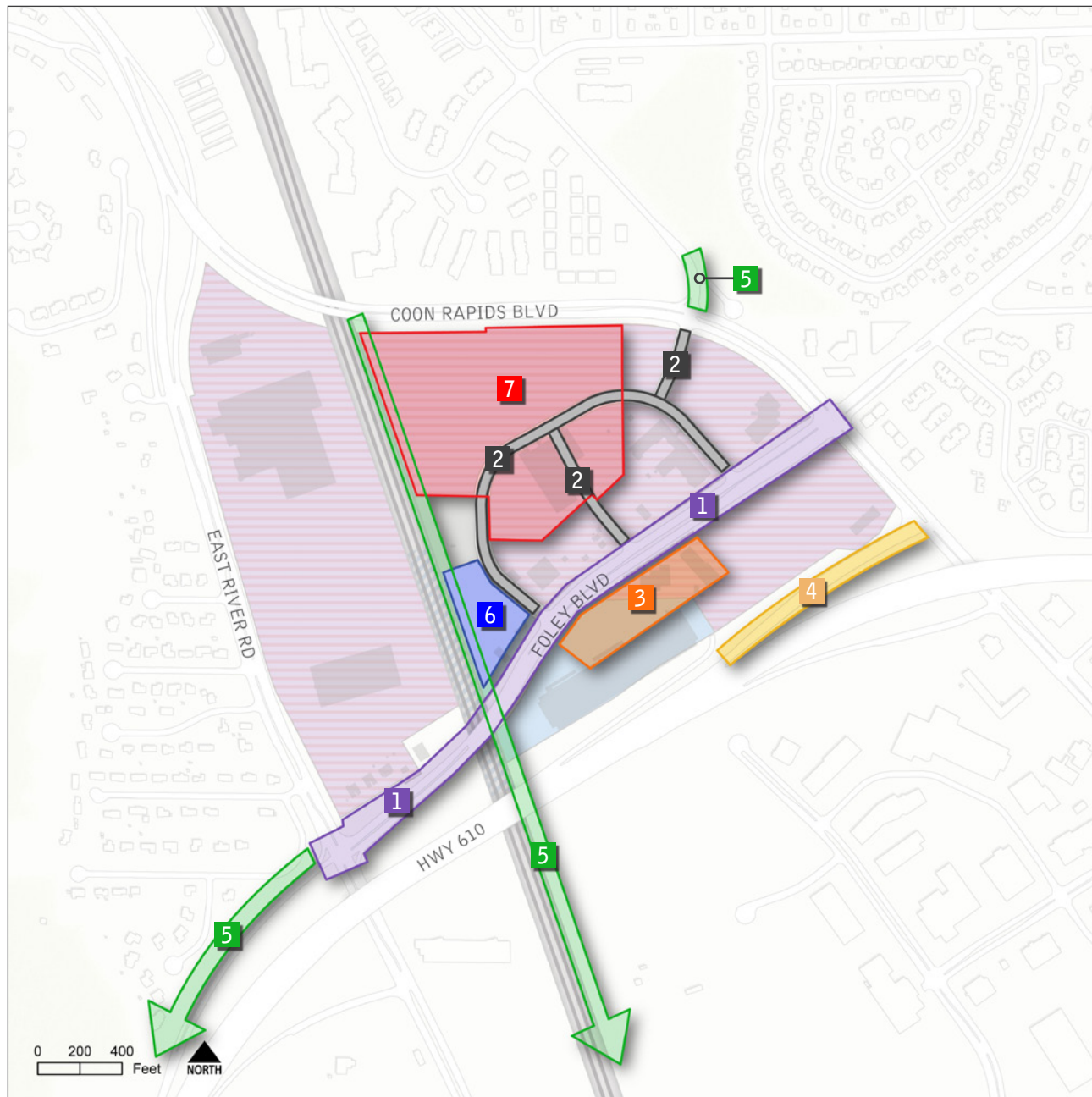
Northern Lights Express (NLX) has identified Foley as the preferred location for a north metro station. The City should work with Anoka County (who owns the property) and NLX on the development of the transit station area north of Foley Boulevard, including a public plaza that can serve as a key gathering spot. Development of the transit station for NLX should be carefully considered so as to ensure a future Northstar station continues to be possible.

**7 Clean up of Berry Plastics Site**

The Berry Plastics Site is an identified superfund site that is in need of environmental remediation. The property is currently available for sale and has been slated to be cleaned up by an outstate firm. The City should be in proactive discussions with the Minnesota Pollution Control Agency, the property owners, and the firm cleaning up the site about the cleanup process. The City should also be exploring with Coon Creek Watershed partnerships and opportunities for innovative stormwater management of the wetlands that would enable more of the site to be used to create employment opportunities in the community.



Figure 4.5 - Public Sector Investment Priorities



## FUNDING TOOLS

## FUNDING TOOLS

Redevelopment can be challenging, particularly when there is environmental contamination, land to assemble, or new infrastructure to construct. The tables on pages 57-59 outline various potential funding sources for infrastructure, development and other improvements.

## Potential Funding Sources

PROGRAM NAME	ADMINISTERING AGENCY	TYPE OF ASSISTANCE	FUNDS AVAILABLE	DESCRIPTION
LCDA pre-development	Met Council	Grant	-	Assist cities with detailed redevelopment designs and economic feasibility
LCDA Development	Met Council	Grant	-	Public infrastructure and land assembly
LCA-TOD Pre-development	Met Council	Grant	\$100,000	Site Plans, design workshops, appraisals, market studies, impact analyses, soil testing, pro-forma development in support of a future development project. *Must have associated development project
LCA-TOD Development	Met Council	Grant	\$2,000,000	Site Acquisition, Holding Costs, Publically-Accessible Infrastructure, Placemaking. *Must have associated development project
TBRA-TOD Cleanup	Met Council	Grant	\$1,000,000	Cleanup of contaminated soil, groundwater, soil vapor or asbestos-containing materials and lead-based paint. *Must have associated development project
Local Housing Incentives Account	Met Council	Grant	\$1,500,000 total	Gap financing costs such as land/property/structure acquisition, demolition, site preparation, general construction/structural additions, alterations and rehabilitation, interior and exterior finishing, roofing, electrical, plumbing, heating and ventilation. *Must have associated development project
Tax Base Revitalization Account	Met Council	Grant	\$5,000,000 total	Phase 1 and 2, RAP development, demolition and site prep, soil/ groundwater remediation, soil vapor mitigation, asbestos abatement, LBP removal for projects with serious redevelopment potential.
Transportation Alternatives Program	Met Council - Transportation Advisory Board	Grant	\$6,000,000 total	Construction of infrastructure for bicycle and pedestrian facilities, Safe Routes to School Infrastructure, scenic and environmental improvements, and streetscape enhancements for projects implemented 2017/18
Regional Transportation Solicitation	Met Council - Transportation Advisory Board	Grant	-	Three evaluation categories, grouped by mode: Roadways, Transit, and Bicycle/Pedestrian, for projects implemented beginning 2018/19
Surface Transportation Program	Met Council	Grant	Up to 80% of project cost	This program is primarily used for roadway reconstruction, but does allow for construction of trails and sidewalks as part of a larger overall project. The program also supports the construction of independent bikeways (generally used for commuting purposes rather than recreational purposes).

## Potential Funding Sources (continued)

PROGRAM NAME	ADMINISTERING AGENCY	TYPE OF ASSISTANCE	FUNDS AVAILABLE	DESCRIPTION
Redevelopment	DEED	Grant	Up to 50% of project's redevelopment costs	Land acquisition, stabilizing soils demolition, infrastructure improvements and ponding or other environmental infrastructure and costs necessary for adaptive reuse of buildings. Eligible costs can only be applied towards publicly owned land with a public purpose, or public infrastructure costs that support a private redevelopment
Contamination Cleanup	DEED	Grant	Up to 75% of project's cost.	Investigation and cleanup costs necessary to implement RAP for projects with serious redevelopment potential.
Contamination Investigation and RAP Development	DEED	Grant	Up to 75% of project's cost. Max \$50,000	Investigation for projects with serious redevelopment potential.
Innovative Business Development Public Infrastructure Program (BDPI)	DEED	Grant	Up to 50% of project cost	Grant funds are to be used to assist Eligible Applicants with complex and costly Public Infrastructure projects when a funding gap exists and alternative sources of public and private financing are not adequate. "Publicly owned infrastructure related to a development project, including projects that target manufacturing, technology, warehousing and distribution; research and development; innovative business incubator; agricultural processing; or industrial, office, or research park development that would be used by an innovative business Funds from this program can also be used for land acquisition and preparation, telecommunications, bridges, parking ramps, demolition, hazard remediation, pre-design, construction, equipment and furnishings." Must have associated development project.
Minnesota Cleanup Revolving Loan Fund	DEED-EPA	Loan	Up to 78% of project costs	Cleanup Owner must be borrower and not the responsible party. Sites acquired after 1/11/2002
Housing Trust Fund (HTF)	Minnesota Housing	Grant	-	Development, construction, acquisition, preservation, and rehab of low-income rental housing. Currently used primarily for rent assistance and operating expenses, but has been available for capital funding previously. *Must have associated development project
Preservation Affordable Rental Investment Fund	Minnesota Housing	Loan	-	Acquisition, rehab, debt restructuring
Highway Safety Improvement Program	MnDOT	Grant	Up to 90% of project cost Max \$2,000,000	"Proactive" projects being solicited for 2014, 2015, and 2016. Examples include active intersection warning systems, ped refuge islands, curb extensions pavement messages, roundabouts, and sidewalks. Priority will be given to applications that are making impacts throughout the network (at multiple locations) or a corridor based approach.
Transportation Revolving Loan Fund	MnDOT	Loan	-	Road and bridge projects, and transit projects. Pre-design, acquisition, maintenance, repair, construction, transit capital purchases, drainage, signs, protective structures. Projects must be eligible under Title 23 or Title 49 or the U.S. Code and Minnesota Statute.
Local Trail Connections Program	MN DNR	Grant	Up to 75% of project cost Max \$150,000	Acquisition and development of trail facilities. Projects must result in a trail linkage that is immediately available for use by the general public. Trail linkages include connecting where people live (e.g. residential areas within cities, entire communities) and significant public resources (e.g. historical areas, open space, parks and/or other trails). Funds used to promote relatively short trail connections between where people live and desirable locations, not to develop significant new trails.
Minnesota Targeted Brownfields Assistance Program	Minnesota Pollution Control Agency	Technical Assistance	In Kind	Technical consultation for Phase I, 2 and cleanup (RAP). Must pose a potential threat with no responsible or voluntary responsible party

## FUNDING TOOLS

## Potential Funding Sources (continued)

PROGRAM NAME	ADMINISTERING AGENCY	TYPE OF ASSISTANCE	FUNDS AVAILABLE	DESCRIPTION
Small Business Environmental Improvement Loan	Minnesota Pollution Control Agency	Loan	\$1,000 to \$50,000	Businesses with fewer than 50 FTE, after tax profits of <\$500,000 and net worth <\$1 million . Investigations, cleanup, capital equipment purchase to reduce/prevent pollution/waste. Collateral required and must demonstrate ability to repay loan
Petroleum Tank Release Clean Up Account-Petrofund	Department of Commerce	Reimbursement	Up to 90% of eligible costs, not to exceed \$1 million per release or \$2 million per facility	Owners/operators of underground and above ground petroleum storage tanks, owners of contaminated properties of leaking tanks Site investigation and cleanup deemed necessary by the MPCA. Minnesota Statutes 115C and Minnesota Rules Chapter 2890
Federal Brownfields Cleanup Program	EPA	Grant	Up to \$200,000 (20% match)	Demolitions, cleanup, removal of some abandoned containers and underground petroleum tanks. Must own the property and cannot be responsible party. 20% matching funds
Federal Brownfields Site Assessment Program	EPA	Grant	Up to \$200,000	Inventory, characterization, Assessment (Phase I or II), remedial planning and VIC program fees. Cannot fund Federal/state Superfund sites, federal lands
Federal Brownfields Cleanup Revolving Loan Grants	EPA	Grants to capitalize RLF	Up to \$1 million	Demolitions, cleanup, removal of some abandoned containers and underground petroleum tanks. Loans/grants cannot be provided to party that caused contamination. 20% match
Joint Development	FTA	Grant	-	Acquisition, demolition, site preparation, utilities, building foundations, walkways, ped and bike access, safety/security facilities, construction of space for commercial uses, parking improvements with a transit justification, project development activities (design, engineering, environmental analysis). Must benefit one of four criterion: Economic benefit; public transportation benefit; fair share of revenue; fair share of costs





